

CHAPTER 8.

DEVELOPMENT STANDARDS

Division 8.1. General Provisions8-2
 Sec. 8.1.1. Applicability 8-2

Division 8.2. Development Bonuses8-3
 Sec. 8.2.1. Affordable Housing 8-3
 Sec. 8.2.2. Open Space 8-3

Division 8.3. Site Access and Parking8-4
 Sec. 8.3.1. Blocks and Connectivity 8-4
 Sec. 8.3.2. Vehicle Access 8-10
 Sec. 8.3.3. Pedestrian Access 8-13
 Sec. 8.3.4. Vehicle Parking 8-19
 Sec. 8.3.5. Vehicle Parking Design. 8-25
 Sec. 8.3.6. Vehicle Use Areas. 8-29
 Sec. 8.3.7. Bicycle Parking 8-32

Division 8.4. Transitions and Landscaping8-35
 Sec. 8.4.1. Transitions 8-35
 Sec. 8.4.2. Transition Screens 8-43
 Sec. 8.4.3. Frontage Screens 8-48
 Sec. 8.4.4. Site Element Screens 8-53
 Sec. 8.4.5. Fences and Walls 8-56
 Sec. 8.4.6. Retaining Walls 8-65
 Sec. 8.4.7. Plants and Plant Material 8-67

Division 8.5. Signs8-70

Division 8.6. Lighting 8-115
 Sec. 8.6.1. Outdoor Lighting 8-115

GENERAL PROVISIONS**DIVISION 8.1. GENERAL PROVISIONS****Sec. 8.1.1. Applicability**

- A. Development standards apply based on the proposed project activity, as specified in the table below. The development standards apply to all zoning districts as conveyed in the table below:

Development Standards		Project Activity						
		New Construction	Addition	Site Modification	Facade Modification	Change of Use	Renovation	Maintenance and Repair
Development Bonuses	<i>Div. XX.</i>							
Affordable Housing	<i>Sec. XX.</i>	●	●	○	○	●	●	○
Open Space	<i>Sec. XX.</i>	●	●	●	○	●	●	○
Site Access and Parking	<i>Div. XX.</i>							
Blocks and Connectivity	<i>Sec. XX.</i>	●	○	○	○	○	○	○
Vehicle Access	<i>Sec. XX.</i>	●	○	●	○	○	○	○
Pedestrian Access	<i>Sec. XX.</i>	●	●	●	○	○	○	○
Vehicle Parking	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Vehicle Parking Design	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Vehicle Use Areas	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Bicycle Parking	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Transitions and Screening	<i>Div. XX.</i>							
Transitions	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Frontage Screening	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Site Element Screening	<i>Sec. XX.</i>	●	●	●	○	●	○	○
Fences and Walls	<i>Sec. XX.</i>	●	○	●	○	●	○	○
Retaining Walls	<i>Sec. XX.</i>	●	○	●	○	●	○	○
Plants and Plant Material	<i>Sec. XX.</i>	●	●	●	○	○	○	○
Signs	<i>Div. XX.</i>	●	●	○	●	●	●	○
Lighting	<i>Div. XX.</i>	●	○	●	○	○	○	○

KEY: ● = Standards generally apply ○ = Standards do not apply

- B. Where a standard is listed as applying, all applicable standards must be met. The applicable standards may be further modified by the applicability provisions for each development standard. For existing buildings and uses, applicability may also be modified by **Chapter 9. Nonconformities**.
- C. Project activity is defined in **Chapter 10. General Standards and Definitions**.

DIVISION 8.2. **DEVELOPMENT BONUSES**

Sec. 8.2.1. **Affordable Housing**

[Note: Section forthcoming in Module III]

Sec. 8.2.2. **Open Space**

[Note: Section forthcoming in Module III]


DIVISION 8.3. **SITE ACCESS AND PARKING**

Sec. 8.3.1. **Blocks and Connectivity**

A. Intent

To provide a well-connected street network with safe and convenient vehicular and pedestrian access within developments and between adjacent developments.

B. Applicability

1. Blocks and connectivity requirements apply to any development site meeting one of the following standards:
 - a. Development sites greater than 2 acres.
 - b. Development sites greater than 600 feet in any dimension.
2. The Director may waive block perimeter and block length requirements for portions of the site that abut and are located within 600 feet of obstructions to the street network that are permanent and inaccessible. Inaccessible obstructions include steep slopes in excess of 25%, freeways, railroads, sensitive utility easements, and other similar obstructions where pedestrian access to the obstructing land use is detrimental or prohibited, and the future provision of a complete block is infeasible.
3. The Director may increase maximum block length standards up to 1,200 feet for portions of the site that abut and are located within 600 feet of obstructions to the street network that are permanent and accessible. Accessible obstructions include public school campuses, public parkland, waterways, stream buffers, cemeteries, and other similar obstructions.

C. Standards

1. Block Perimeter and Length

- a. A block must be bound by streets on all sides. The Director may allow one side of the block to abut a park or similarly accessible open space.
- b. No development is permitted on an applicable site having a block perimeter greater than 1,600 feet or a block length greater than 600 feet.
- c. Where the existing block perimeter is greater than the maximum allowed block perimeter or block length, development projects must provide new streets and street rights-of-way to comply with the maximum block standards.
- d. Where all lots on a block have alley access, the maximum block perimeter and block length may be increased by 25%.

2. Mid-Block Pedestrian Passageways

- a. Where the block is bisected by a pedestrian connection that meets the following standards, the maximum block perimeter and block length may be increased by 25%:

#001

Posted by **Selena Xayavong** on **12/05/2024** at **9:36am** [Comment ID: 598] - [Link](#)

Agree: 0, Disagree: 0

Does this align with DRI requirements?

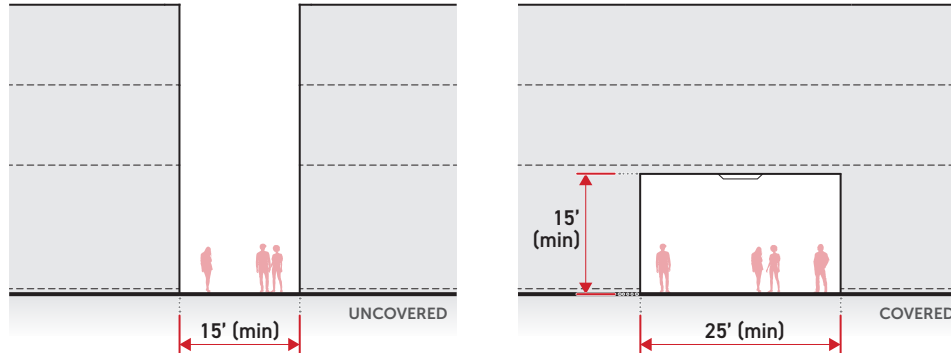
Reply by **SiteAdmin** on **12/06/2024** at **2:58pm** [Comment ID: 644] - [Link](#)

Answer

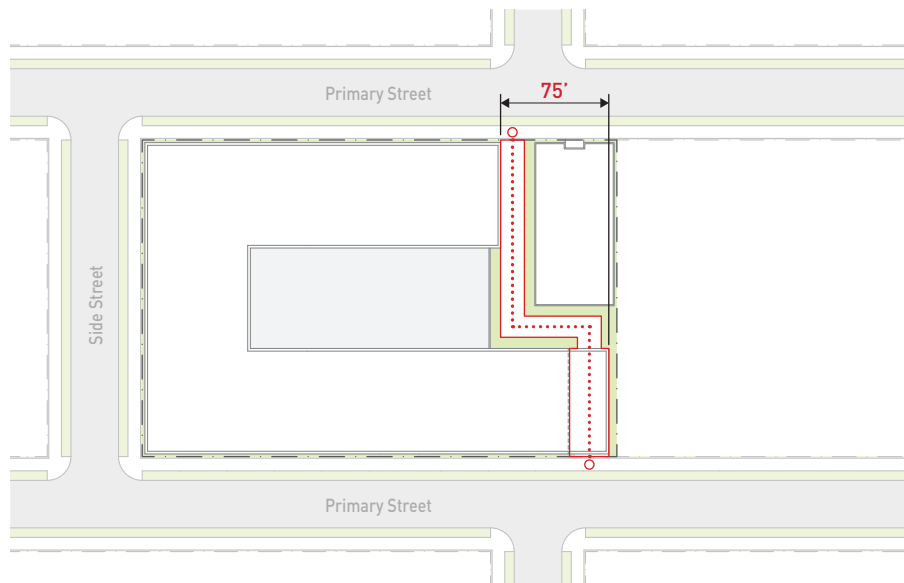
Agree: 0, Disagree: 0

No. DRIs are not based on acreage, they are based on development program.

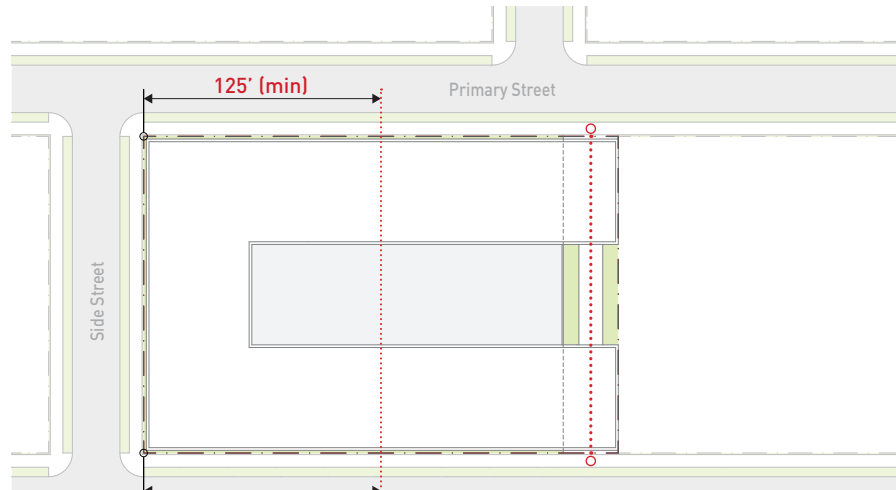
- i. The pedestrian passageway must connect from the public sidewalk on the primary street through the lot to either the public sidewalk, alley, other public way, or easement abutting the opposing lot line.
- ii. The pedestrian passageway must be a minimum width of 15 feet for uncovered portions and a minimum width of 25 feet for covered portions.
- iii. The pedestrian passageway must have a minimum clear height of 15 feet, with the exception of required luminaries and trees.



- iv. The centerline of the pedestrian passageway cannot vary laterally more than 75 feet from the access point on the primary street, measured parallel to the primary street lot line.



- v. Access to the pedestrian passageway must be a minimum of 125 feet from the street intersection, measured along the primary street.

SITE ACCESS AND PARKING

- vi. The pedestrian passageway must be physically separated from and uninterrupted by motor vehicle use areas except where required to cross a drive-aisle. Drive-aisle crossings must be the shortest practical distance.
 - vii. The pedestrian passageway must be illuminated in accordance with **Div. XX - Lighting**.
 - viii. The pedestrian passageway must be made permanently available to the general public, at no cost, between sunrise and sunset daily, or during the operating hours of the building, whichever would result in a longer period of time. No gates or other barriers may block any portion of the pedestrian passageway from pedestrian access during the required available hours.
- b. Pedestrian passageways may count towards required outdoor amenity space if they meet the requirements of **Sec. XX - Outdoor Amenity Space**.

3. New Streets

Where a new street is provided, it must meet the following standards:

- a. New streets may be public or private, but must function as public streets.
- b. New streets must connect two other streets.
- c. New streets are not permitted to be gated.
- d. New streets must be coordinated with other existing or planned streets, and must intersect with existing streets at existing intersections, wherever practical, as determined by the Director.
- e. Where land ownership prevents a through street, a street stub must be provided to enable a future through street connection. The street stub must be located at the elevation of existing grade along abutting parcels, as determined by the Director.
- f. If a street stub exists on an abutting parcel, the street network provided by any new development abutting the street stub must connect to the stub street and complete the alignment as a through street wherever practical, as determined by the Director.

#002

Posted by **LarryA** on **02/07/2025** at **5:18am** [Comment ID: 1046] - [Link](#)

Question

Agree: 0, Disagree: 0

Is this section in reference to new developments/neighborhoods to make their streets gridded rather than unconnected as many are today?

Reply by **SiteAdmin** on **02/07/2025** at **4:59pm** [Comment ID: 1050] - [Link](#)

Answer

Agree: 0, Disagree: 0

This standard seeks to ensure that large development sites incorporated new streets that form an interconnected network. A grid pattern, per se, is not required. Many parts of Atlanta have connected streets that are curvilinear in layout. Ansley Park and Inman Park are examples of this.

#003

Posted by **Laurel David** on **01/24/2025** at **2:58pm** [Comment ID: 967] - [Link](#)

Question

Agree: 0, Disagree: 0

for d, e and f - shouldn't ATL DOT be given priority in making these decisions?

Reply by **SiteAdmin** on **01/28/2025** at **4:52pm** [Comment ID: 987] - [Link](#)

Answer

Agree: 0, Disagree: 0

The Zoning Ordinance is administered by the Director, who may consult with other City Departments before rendering decisions. We will explore adding such a notation for this provision.

- g. Cul-de-sacs are only allowed only where the Director determines that topographical, ownership, or other challenges beyond the land developer's control prevent the creation of a regular street grid.
- h. New streets must be developed in accordance with the standards in **Div. XX - Streetscapes** and Atlanta Department of Transportation standards in the *Streets Atlanta: A Design Manual for Multimodal Streets* and *City of Atlanta Public Right-of-Way Manual*.

4. Cross-Access

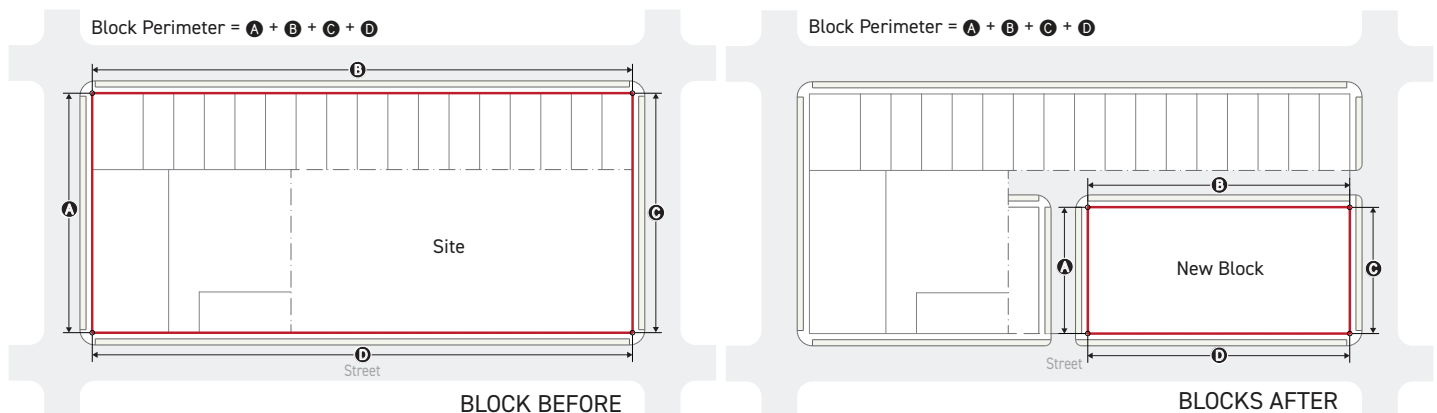
Where no new streets are provided, and abutting lots have compatible adjacent uses, as determined by the Director, the following standards for cross-access are required:

- a. Projects must design internal vehicular circulation to allow for future cross-access between abutting lots, wherever practical, as determined by the Director.
- b. No project is allowed to prohibit or obstruct the ability to provide future vehicle, bicycle, and pedestrian access and connectivity to adjacent properties, unless the Director determines it is impracticable due to legal, topographic, or other site-specific constraints.
- c. Cross-access standards do not require inter-parcel access to be granted to adjacent property owners.

D. Measurement

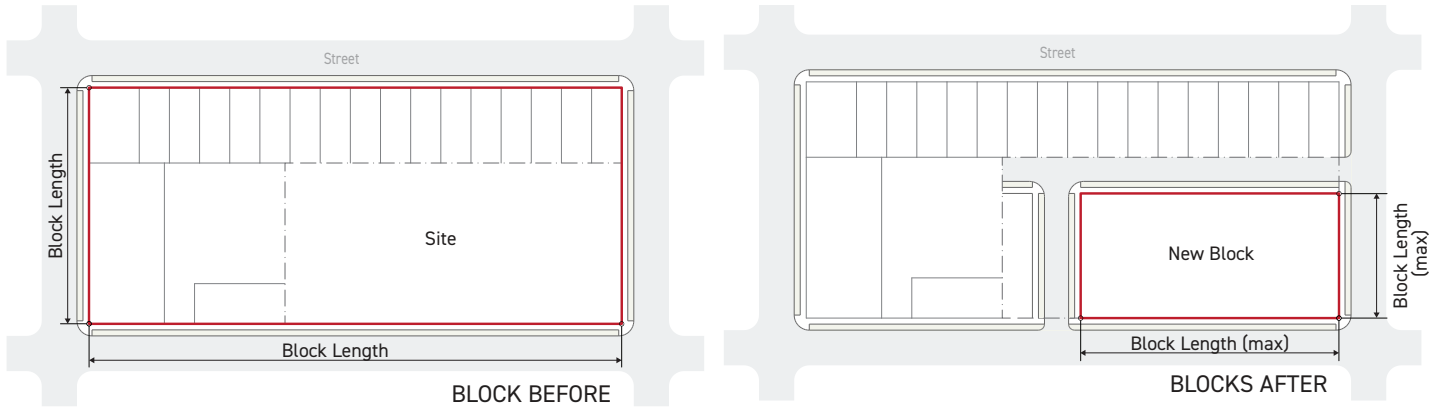
1. Block Perimeter

Block perimeter is measured along all lot lines abutting a street.



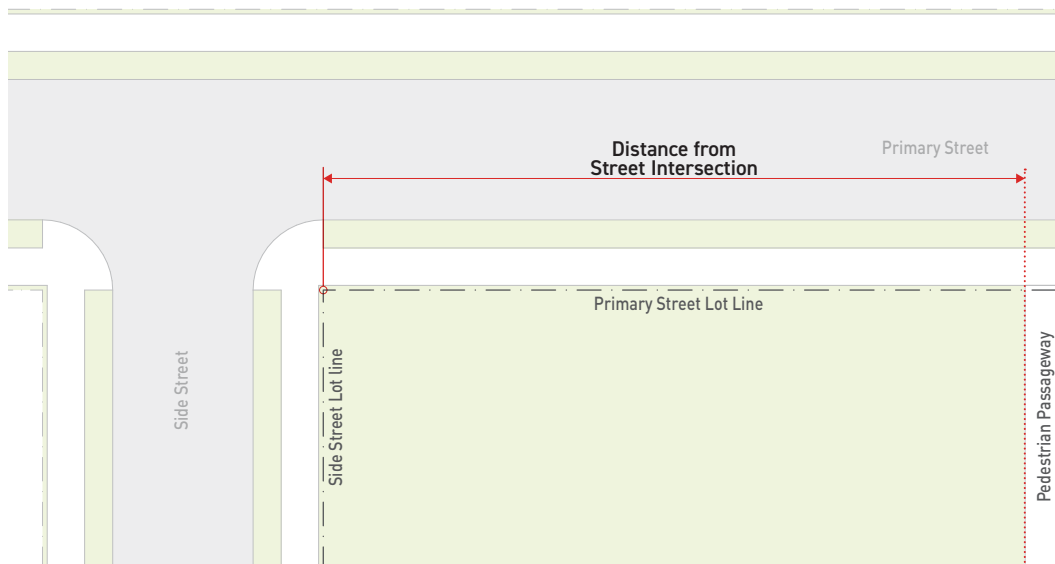
2. Block Length

Block length is the length of each side of the block, measured along each street lot line along the block face.



3. Pedestrian Passageways

- Pedestrian passageway width is measured from one edge of the designated passageway perpendicularly to the opposite edge of the passageway.
- Pedestrian passageway distance from street intersection is measured from the point where two street lot lines intersect adjacent to a street corner to the nearest edge of a required pedestrian passageway, measured along the street lot line.



- Variation in a pedestrian passageway is measured parallel to the primary street lot line from one edge of the passageway access point to the furthest edge of the opposing passageway access point.

E. Relief

1. A change of up to 004 from blocks and connectivity standards may be granted in accordance with Sec. XX. Administrative Adjustment.
2. A change beyond 10% may be granted in accordance with Sec. XX. Variance.

005

#004

Posted by **Laurel David** on **01/24/2025** at **3:00pm** [Comment ID: 968] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

maybe state 10% of what?

Reply by **SiteAdmin** on **01/28/2025** at **4:55pm** [Comment ID: 988] - [Link](#)

Answer

Agree: 0, Disagree: 0

All Relief provisions will be updated and clarified following release of Module III.

#005

Posted by **haney8604** on **12/17/2024** at **4:53pm** [Comment ID: 768] - [Link](#)

Question

Agree: 0, Disagree: 0

This % is the same in each of the relief sections in this chapter - better to have it as it's own section?

Reply by **SiteAdmin** on **12/18/2024** at **1:39pm** [Comment ID: 785] - [Link](#)

Answer

Agree: 0, Disagree: 0

Relief is not the same everywhere, especially in other chapters. There are some situations where it is other than 10%, or where use of a percent isn't applicable to the relief. An example of the latter would be areas with existing steep topography, which (today) qualify for greater relief in order to reduce grading impacts.

Sec. 8.3.2. Vehicle Access

A. Intent

To ensure motor vehicle access to sites is designed to support the safety of all users by minimizing conflicts with pedestrians, cyclists, transit vehicles, micro-mobility devices, and motor vehicles on the abutting public right-of-way and to avoid detrimental effects on the surrounding public realm, while providing sufficient access to vehicle parking and other motor vehicle use areas.

B. Applicability

Any lot that provides access for motor vehicles.

C. Standards

1. General

- a. All existing and proposed development that provides on-site parking or loading must provide a means of vehicle access to and from a street or alley.
- b. Driveways must be perpendicular to the street. Driveways are not permitted parallel to the street between the sidewalk and a building.
- c. Circular driveways or drop-off lanes are not permitted between any building and the street, with the exception of general lodging and hospital uses in any district and general household living uses in Residential (R-) Form Districts.
- d. When a lot is adjacent to a primary and side street without an alley, vehicle access must be placed along the side street unless otherwise permitted by the Director.
- e. When an improved alley exists or is proposed by the development project, vehicle access must take place from the alley unless otherwise permitted by the Director.
- f. Access may be shared between abutting lots, provided an easement, deed restriction or other similar agreement acceptable to the City is provided.
- g. All vehicle access designs must be approved by the Director and must conform to the provisions of the *Streets Atlanta: A Design Manual for Multimodal Streets*, and *City of Atlanta Public Right-of-Way Manual*.

2. Driveway Design

- a. Every platted lot is permitted at least one driveway unless otherwise modified by the Director.
- b. Where feasible, driveways must be at least 100 feet from the nearest adjacent intersection or driveway along the same block face unless otherwise permitted by the Director.

#006

Posted by **haney8604** on **12/17/2024** at **4:41pm** [Comment ID: 765] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Please remove "where feasible." This language gives developers AND the city too much leeway to create bad urban design, dangerous intersections, and increased traffic. There are too many instances of commercial driveways being WAY to close to intersections - see the QT at Northside Dr/10th, which the city never should have allowed. Each instance should be considered on a case by case basis and be required to apply for variance.

Reply by **SiteAdmin** on **12/18/2024** at **1:52pm** [Comment ID: 790] - [Link](#)

Answer

Agree: 1, Disagree: 0

Thanks for your comment. Driveway placement is regulated by zoning and the agency with jurisdiction over the street. In the case of Northside Drive, that is GDOT. The City cannot require a developer to obtain a variance to its zoning when such variance is required by the State of Georgia. In the case of 10th, which is a City street, we can explore strengthening this. Condition C.1.d above, which requires use of side streets for driveways would help (it would have required the driveway to be off of Fielder Ave instead of 10th.

Reply by **haney8604** on **12/19/2024** at **2:03pm** [Comment ID: 792] - [Link](#)

Agree: 0, Disagree: 0

Thanks for the response. That would have been great in the instance (would also be nice if the city would stop ignoring my requests for a traffict study at the intersection with the goal of adding a curb between east and west lanes on 10th to prevent people turning across lanes into the QT... but i digress. Thanks again!

#007

Posted by **Laurel David** on **01/24/2025** at **3:06pm** [Comment ID: 969] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

rather than using "side street", it might be better to require access on the street with the lower road classification. For example, the side street could be an arterial road and the primary a local road. In this case, it is typically better to allow curb cuts on the local rather than the arterial road, unless ATL DOT prefers otherwise.

Reply by **SiteAdmin** on **01/28/2025** at **4:56pm** [Comment ID: 989] - [Link](#)

Answer

Agree: 0, Disagree: 0

Great point. Thanks!

#008

Posted by **Laurel David** on **01/24/2025** at **3:10pm** [Comment ID: 970] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Director of OZD? or ATL DOT

I made this comment above. If the decision in the code rests with the Director of OZD, it could conflict with ATL DOT, which may mean having to make modifications to the site plan at the permitting stage. The earlier ATL DOT can be consulted in the approval process regarding access to the public road network, the more efficient it will be

Reply by **SiteAdmin** on **01/28/2025** at **4:58pm** [Comment ID: 990] - [Link](#)

Answer

Agree: 0, Disagree: 0

As drafted, this is OZD, but we will look into clarifying this. Thanks!

#009

Posted by **Michael Barnett** on **03/31/2025** at **8:35pm** [Comment ID: 1160] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

When I specifically think about the alley between Peachtree Street and Peachtree Circle which is used by single family homes on the Peachtree Circle side, I'm hard pressed to understand how a new condo tower with 300 cars could use that alley as it's sole access point for cars. That alley would need to be improved to the level of a one-way street to handle that volume of traffic.

Reply by **SiteAdmin** on **04/02/2025** at **2:05pm** [Comment ID: 1182] - [Link](#)

Answer

Agree: 0, Disagree: 0

This is intended to gently encourage the use of alleys, but provided easy relief, if not feasible, consistent with current practice. The Director would consider the suitability of the alley in rendering their decision.

#010

Posted by **Michael Barnett** on **03/31/2025** at **8:38pm** [Comment ID: 1161] - [Link](#)

Question

Agree: 0, Disagree: 0

I'm confused by this. Doesn't the code state that where an alley exists vehicular access must be from the alley? How does that marry with the right to a curb-cut?

Reply by **SiteAdmin** on **04/02/2025** at **2:10pm** [Comment ID: 1183] - [Link](#)

Answer

Agree: 0, Disagree: 0

It does not. it says if an approved alley exists or is proposed, then access should be provided. Many parts of the city are very suburban in form and lack alleys.

- c. The maximum number of permitted driveways are as follows:

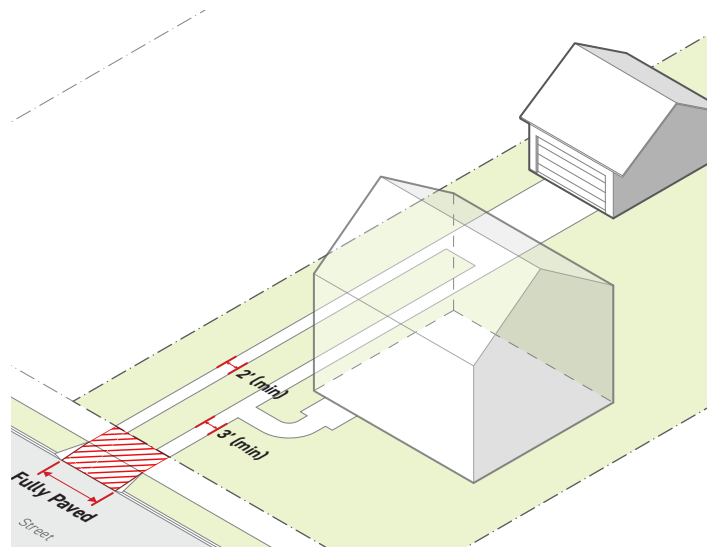
# of Abutting Streets	Street Lot Line Length	Curb Cuts per Street (max)	Driveway Spacing (min)
1	Up to 300'	1	N/A
1	Above 300'	2	200'
2	Any	1 per street	N/A
3 or more	Any	1 per street	N/A

- d. Driveway widths must meet the following standards:

	Number of Lanes per Driveway (max)	Lane Width (min/max)	
		Primary Street	Side Street
Driveway serving up to 6 on-site parking spaces	1	8' / 10'	8' / 20'
Driveway serving more than 6 on-site parking spaces	2	8' / 12'	8' / 12'
Driveway serving an Industrial Use	2	12' / 20'	12' / 20'

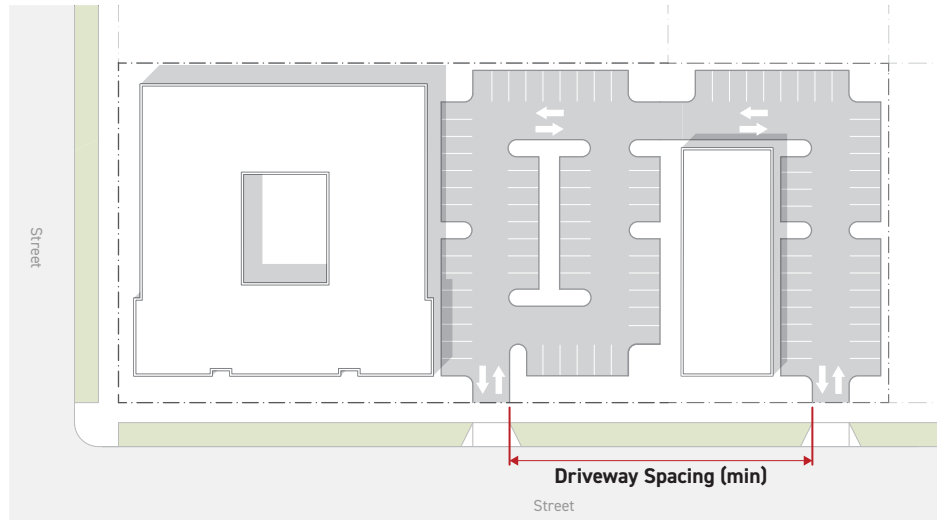
3. Ribbon Driveways

- For driveways serving up to 6 on-site parking spaces, a ribbon driveway is allowed as an alternative to a fully paved driveway.
- The concrete strips in a ribbon driveway must be at least 2 feet in width. When the ribbon is also used as a walkway, the width of the strips must be increased to at least 3 feet.
- Within the right-of-way, the driveway must be fully paved along its total width.



D. Measurement

1. Driveway spacing is measured along the primary or side street boundary line from edge of pavement to edge of pavement from all existing and proposed driveways located on-site and any adjacent driveway or intersection.



2. Lane width is measured separately for each driveway lane.
3. Lane width is measured for each driveway lane from edge of designated lane to edge of designated lane along the street boundary line.

E. Relief

1. A change of up to 10% from vehicle access standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.3.3. Pedestrian Access

A. Intent

To promote walkability by:

1. Improving pedestrian access from the public realm to the interior of buildings;
2. Ensuring that required entrances are conveniently and effectively accessible to pedestrians; and
3. Activating the public realm with building access points and improve convenient pedestrian circulation through large sites to an extent and frequency appropriate to the context.

B. Applicability

Pedestrian access standards apply to all street-facing entries required by the zoning district, based on requirements in **Sec. XX. Windows and Doors**. Pedestrian access standards do not apply to non-required entries.

C. Standards

1. General

Pedestrian access meeting the following standards must be provided based on the zoning district.

Zoning District	Pedestrian Access Type
House-Scal011rm Districts	
All uses	None Required
Neighborhood-Scale Form Districts	
Residential uses	Type 2
All other uses	Type 1
Urban General Form Districts	
All uses	Type 1
Urban Core Form Districts	
All uses	Type 1
Workplace Flex Form Districts	
Industrial uses	Type 3
All other uses	Type 1
Workplace Form Districts	
All uses	Type 3
Special Form Districts	
All uses	Type 2

#011

Posted by **Jim Winer** on **12/08/2024** at **3:38pm** [Comment ID: 663] - [Link](#)

Agree: 0, Disagree: 0

Single family residences have been permitted in the City without a walkable connection from the driveway or the public sidewalk to the front door. This has been done in cases where the lot coverage was maximized. After the certificate of occupancy is granted, the owner will add a sidewalk. Consider a requirement that addresses this issue.

Reply by **SiteAdmin** on **12/09/2024** at **11:15am** [Comment ID: 698] - [Link](#)

Answer

Agree: 0, Disagree: 0

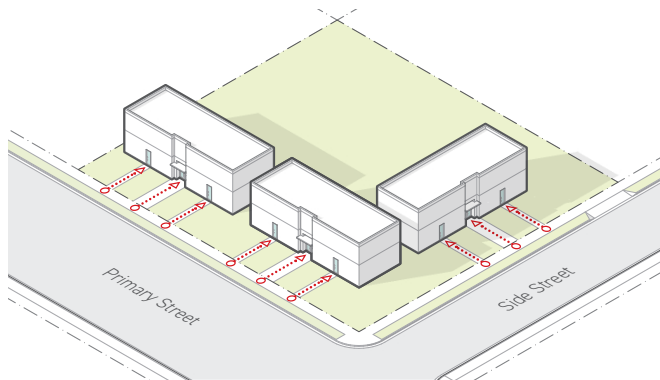
Thank you for your comment.

As a reminder, "House-Scale" districts only include R1 through R3 today. These districts currently have no sidewalk requirements.

2. Pedestrian Access Types

PEDESTRIAN ACCESS TYPE 1

Intended to ensure buildings are highly integrated with the surrounding public realm and to promote walking as a safe and convenient mobility option to improve connectivity through large sites.

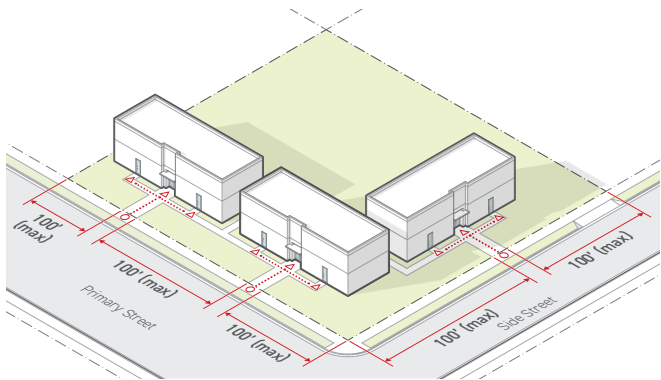


DIMENSIONAL STANDARDS

Pedestrian connection type	Direct
Pedestrian connection spacing (max)	1 per street-frontage entry

PEDESTRIAN ACCESS TYPE 2

Intended to ensure buildings are conveniently accessible from the public realm and to promote walking as a safe and convenient mobility option to improve connectivity through very large sites.

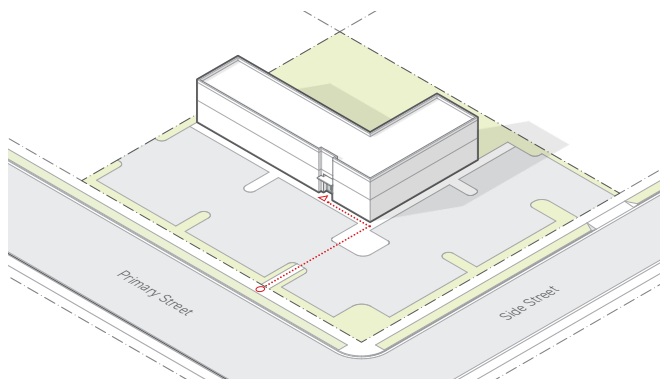


DIMENSIONAL STANDARDS

Pedestrian connection type	Linked
Pedestrian connection spacing (max)	100'

PEDESTRIAN ACCESS TYPE 3

Intended to ensure buildings are accessible from the public realm by requiring physical connection between the public realm and the interior of a building.



DIMENSIONAL STANDARDS

Pedestrian connection type	Linked
Pedestrian connection spacing (max)	1 per lot

#012

Posted by **Laurel David** on **01/24/2025** at **3:14pm** [Comment ID: 971] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Why not allow linked access to attached homes? it would reduce the amount of impervious surface and be more sustainable even if a max of three could be linked

3. Pedestrian Connection

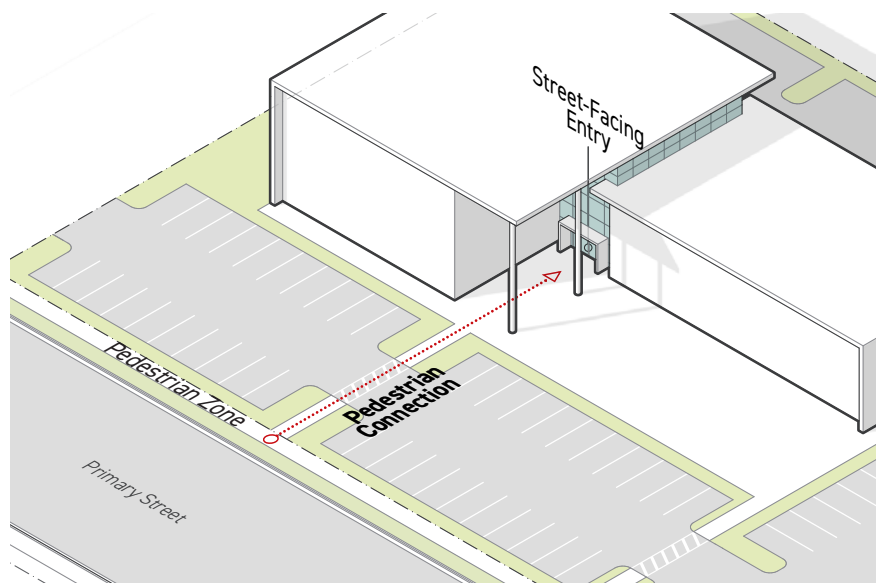
a. General

Pedestrian connections provides pedestrian access from the public right-of-way to street-facing building entries. The pedestrian access type requires either a direct or linked pedestrian connection.

b. Linked Pedestrian Connection

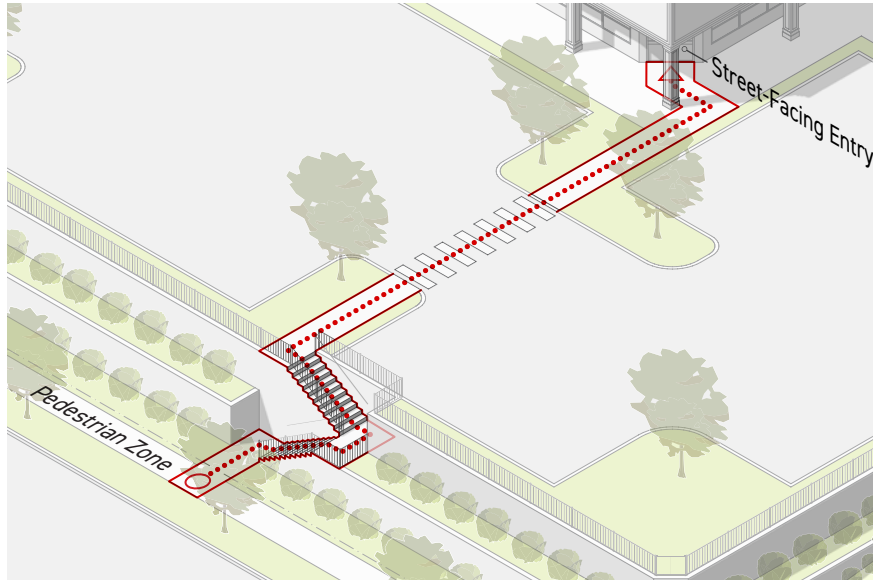
Where the pedestrian access type requires a linked pedestrian connection, the pedestrian connection must meet the following standards:

- i. Linked pedestrian connections must connect to the sidewalk, multi-use trail, or other publicly accessible pedestrian-facility along the street lot line, to a required street-facing entrance.
- ii. A minimum of 1 pedestrian connection must be provided for each street lot line where a street-facing entry is required.
- iii. The pedestrian connection must be at least 5 feet wide.
- iv. The finished surface must be stable, firm, and slip resistant in accordance with Americans with Disabilities Act (ADA) Accessibility Standards for floor surfaces.
- v. The pedestrian connection must be physically separated from and uninterrupted by motor vehicle use areas except where required to cross a drive-aisle. Drive-aisle crossings must be the shortest practical distance.



SITE ACCESS AND PARKING

- vi. If a retaining wall, open drainage, or similar obstacle interrupts a required connection, the connection must be designed to facilitate passage through or over the obstacle. If the connection is not ADA compliant, a separate, compliant connection must be provided.



- vii. If a linked pedestrian connection exceeds 30 feet in length through a parking lot, trees spaced at 30 feet on-center must be provided adjacent to the pedestrian connection.

c. Direct Pedestrian Connection

Where the pedestrian access type requires a direct pedestrian connection, the pedestrian connection must comply with all linked pedestrian connection standards, in addition to the following standards:

- i. A minimum of 1 pedestrian connection must be provided to each street-facing entry required by the zoning district.
- ii. The connection to the public sidewalk must be within 25 feet of the center of the street-facing entry, measured parallel to the applicable lot line.

013

#013

Posted by **haney8604** on **12/17/2024** at **4:45pm** [Comment ID: 766] - [Link](#)

Question

Agree: 0, Disagree: 0

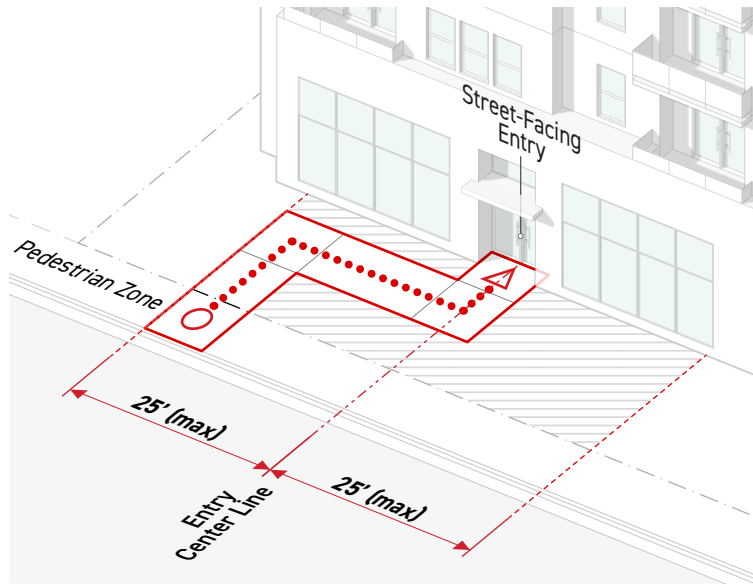
This distance seems arbitrary. What's the scenario this is trying to prevent?

Reply by **SiteAdmin** on **12/18/2024** at **1:48pm** [Comment ID: 788] - [Link](#)

Answer

Agree: 0, Disagree: 0

The purpose is to minimize indirect connections between the door and the sidewalk and reduce the distances non-drivers must travel to access the building.

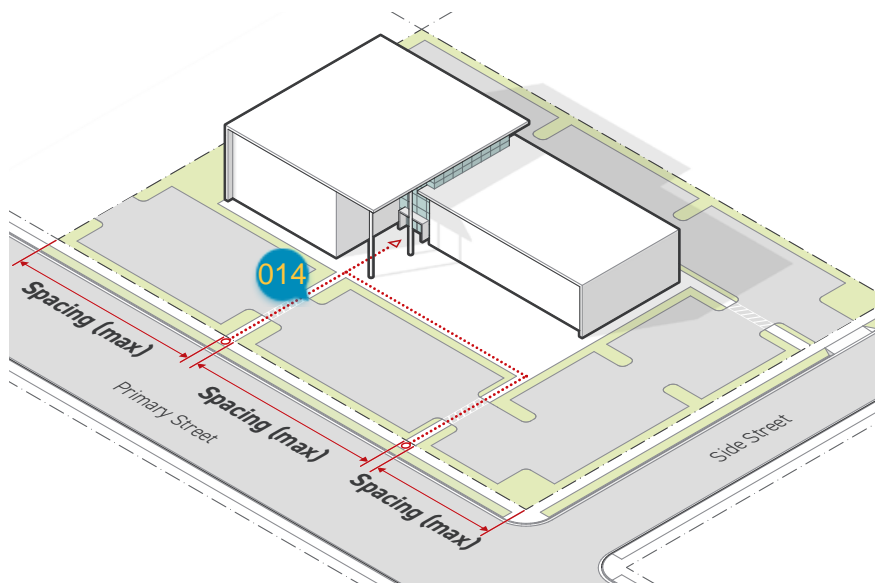


4. Pedestrian Connection Spacing

A required pedestrian connection must not be separated from another pedestrian connection or the end of a street lot line by a distance greater than the maximum allowed pedestrian connection spacing.

D. Measurement

1. Pedestrian accessway width is measured from one edge of the accessway perpendicularly to the opposite edge.
2. Pedestrian accessway spacing is measured as the distance between pedestrian accessways measured following the geometry of the street lot line from the end of the lot line to the edge of the pedestrian accessway and from the edge of the pedestrian accessway to the edge of the adjacent pedestrian accessway.



#014

Posted by **AntonGudiswitz** on **12/15/2024** at **10:24pm** [Comment ID: 749] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Really appreciate this pedestrian connection requirement

E. Relief

1. A change of up to 10% from pedestrian access standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.3.4. Vehicle Parking

A. Intent

To reduce parking demand, support the use of alternative forms of transportation, promote reuse and redevelopment of existing buildings, reduce the overall cost of construction and development, and increase the overall efficiency and use of taxable land within the City.

B. Applicability

1. Vehicle parking standards apply to all lots and the allowed uses in **Ch. XX. Use Districts**.
2. Buildings built before 1965 are not required to meet minimum parking requirements unless the building contains a use over 1,200 square feet holding an alcohol license, according to **City of Atlanta Code of Ordinances, Chapter 10**, in which case only that use is required to meet the minimum parking requirement.

C. Standards

1. Required Vehicle Parking

015
016

- a. Vehicle parking standards are determined by the parking zone applied to the lot according to the Parking Zone Map (**Appendix X**), in accordance with the required vehicle parking table.
- b. Where a minimum parking standard is provided, all uses must, at a minimum, provide the required number of parking spaces.
- c. Where a maximum parking standard is provided, uses cannot exceed the maximum allowed number of parking spaces on-site, unless otherwise permitted.
- d. Where there are multiple uses on a lot, the parking requirements are calculated separately for each use.
- e. The vehicle parking requirement for a use not specifically listed in the table will be as required for the closest similar use, as determined by the Director.
- f. Vehicle parking must be provided as follows:

#015

Posted by **Jim Winer** on **12/08/2024** at **3:41pm** [Comment ID: 664] - [Link](#)

Suggestion

Agree: 1, Disagree: -1

There should not be a minimum on site parking requirement of zero for commercial, institutional, or government establishments within 400 feet of a single family residential neighborhood.

Reply by **SiteAdmin** on **12/09/2024** at **11:14am** [Comment ID: 697] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your comment.

#016

Posted by **AntonGudiswitz** on **12/15/2024** at **10:36pm** [Comment ID: 750] - [Link](#)

Suggestion

Agree: 0, Disagree: -1

Parking should NOT be required in any zone.

SITE ACCESS AND PARKING

020
Parking Zones

Use	Zone 1 (PZ1)	Zone 2 (PZ2)	Zone 3 (PZ3)	Zone 4 (PZ4)
Residential Uses				
Household Living				
General Household Living				
Up to 2 dwelling units	Min: -- 017 Max: --	Min: -- 018 Max: --	Min: 0.5 per dwelling unit Max: --	Min: 1 per dwelling unit Max: --
Above 2 dwelling units	Min: -- Max: 2 per dwelling unit	Min: -- Max: 2 per dwelling unit	Min: 0.5 per dwelling unit Max: --	Min: 1 per dwelling unit Max: --
Live-work	Min: -- Max: 2 per dwelling unit	Min: -- Max: 2 per dwelling unit	Min: 0.5 per dwelling unit Max: --	Min: 1 per dwelling unit 021 Max: --
Purpose-built student housing	Min: -- Max: 1 per 2 bedrooms	Min: -- Max: 1 per 2 bedrooms	Min: 1 per 4 bedrooms Max: --	Min: 1 per 2 bedrooms Max: --
Group Living				
General supervised group living	Min: -- Max: 1 per 2 bedrooms	Min: -- Max: 1 per 2 bedrooms	Min: 1 per 4 bedrooms Max: --	Min: 1 per 2 bedrooms Max: --
General unsupervised group living	Min: -- Max: 1 per 2 bedrooms	Min: -- Max: 1 per 2 bedrooms	Min: 1 per 4 bedrooms Max: --	Min: 1 per 2 bedrooms Max: --
Dormitory, fraternity, or sorority	Min: -- Max: 1 per 2 bedrooms	Min: -- Max: 1 per 2 bedrooms	Min: 1 per 4 bedrooms Max: --	Min: 1 per 2 bedrooms Max: --
Emergency shelter	Min: -- Max: 1 per 1,000 SF	Min: -- Max: 1 per 1,000 SF	Min: 1 per 2,000 SF Max: --	Min: 1 per 1,000 SF Max: --
Maternity supportive housing	Min: -- Max: 1 per 2 bedrooms	Min: -- Max: 1 per 2 bedrooms	Min: 1 per 4 bedrooms Max: --	Min: 1 per 2 bedrooms Max: --
Public and Institutional Uses				
Civic				
Religious assembly	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
All other civic uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Private Education				
Day care, private	Min: -- Max: 1 per 400 SF	Min: -- 019 Max: 1 per 400 SF	Min: 1 per 1,200 SF Max: --	Min: 1 per 600 SF Max: --
All other private education uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 200 SF Max: --	Min: 1 per 200 SF 022 Max: --

#017

Posted by **dimnickph** on **01/12/2025** at **2:14pm** [Comment ID: 870] - [Link](#)

Question

Agree: 0, Disagree: 0

It would be interesting to overlay the proposed Parking Zones with the current zoning map to see the magnitude of the change in parking regulations. Most of our neighborhoods were here before MARTA or the Beltline. This proposal alters future streetscapes as fill-in or redevelopment occurs with no change in property use at that location. Today single family homes zoned R-4 in PZ1 have a minimum parking requirement. Once this is enacted, the requirement will be zero if the current house is taken down and a new house is built on that lot. Am I reading this correctly?

Reply by **SiteAdmin** on **01/12/2025** at **2:52pm** [Comment ID: 872] - [Link](#)

Answer

Agree: 0, Disagree: 0

PZ1 is the existing "high-capacity transit" parking exemption area, as well as zoning districts with no parking requirements. Under the high-capacity transit exemption, no parking is required with a 1/2 mile walk of a MARTA station, including R1-R5 zoning district. This has been law for seven years and supersedes the requirements of zoning district. Houses can be built today with no on-site parking in these areas.

#018

Posted by **phiae01** on **12/30/2024** at **3:56pm** [Comment ID: 797] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

I know this is well intentioned, but the reality is that when you have no parking then people park on the street... This makes it a less desirable area to live and difficult for fire and rescue services. This zone is specific to the beltline, yet no transit exists on the beltline and may not for many decades to come.

Reply by **SiteAdmin** on **01/06/2025** at **1:02pm** [Comment ID: 843] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback. The creation of PK2 (for the Beltline Overlay) was a recent action of City Council. For that reason, we do not anticipate many changes to it.

Reply by **dimnickph** on **01/12/2025** at **1:52pm** [Comment ID: 869] - [Link](#)

Question

Agree: 0, Disagree: 0

I am totally confused. Director Holmes stated directly that Sec. 16-36.001 indeed exempted properties Zoned R1-4 from the recent City Council action eliminating minimum parking requirements in the Beltline Overlay. The Parking Zone Map and this table seem to suggest

that parking minimums will be eliminated in the Beltline Overlay even as it passes through single-family residential areas. This is a major change.

Reply by **SiteAdmin** on **01/12/2025** at **2:45pm** [Comment ID: 871] - [Link](#)

Answer

Agree: 0, Disagree: 0

We're sorry for the confusion. We will try to explain this at the Q&A session on Monday 1/13. You're correct that the existing BL Overlay does not technically exempt R1-R4. However, another existing code section (Sec. 16-28.014(12)) allows lots to count on-street spaces (whether marked or not) towards parking requirements. The result is that all R1-R4 citywide has no effective on-site parking requirement, except along streets that are signed as "no-parking." This law has existed for seven years, well before the BL Overlay was adopted. Since then, a handful of new houses have been built without on-site parking, but most builders choose to provide on-site parking in order to secure a higher sales price.

[Link](#)

Reply by **dimnickph** on **01/14/2025** at **9:37am** [Comment ID: 891] -

Question

Agree: 0, Disagree: 0

Okay. I think I have it. The elimination of parking minimums enacted two years ago didn't change the underlying minimum found in the R-4 section of the code because of the exemption from the exemption in 16-36.001. The minimum in the R-4 code section didn't really mean anything because it was negated in most cases by 16.28.014(12) which eliminated minimum parking where there is a public street in front of the house. So you are just cleaning up the code by eliminating the parking requirement in the underlying zoning. Did I hear this correctly last night?

Reply by **SiteAdmin** on **01/16/2025** at **9:44am** [Comment ID: 895] - [Link](#)

Answer

Agree: 0, Disagree: 0

Correct. There is a parking requirement on paper only. Almost all R1-R4 is on a street with on-street parking, except major corridors.

#019

Posted by **phiae01** on **12/30/2024** at **3:59pm** [Comment ID: 798] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

This seems a little crazy - you are restricting parking spots when most parking is for

the EMPLOYEES. Not every place in Zone 2 is easily accessible by transit. AND there are state and federal minimums for the number of care givers per child. In infant rooms it is 1 per 4 kids. Most infant rooms are < 400sq ft but may hold 12 infants. That's 3 adults per 400sq ft.

Reply by **SiteAdmin** on **01/06/2025** at **12:50pm** [Comment ID: 840] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your input. There are already limits on the amount of parking allowed in PZ2, but we will review the numbers based on your feedback.

#020

Posted by **KUA** on **12/05/2024** at **3:51pm** [Comment ID: 618] - [Link](#)

Question

Agree: 0, Disagree: 0

Why are we still requiring parking in this zoning rewrite? On what scientific/documented basis are you coming up with these requirements other than cutting and pasting from old ordinances? This isn't the 1980s anymore.

Reply by **SiteAdmin** on **12/06/2024** at **1:45pm** [Comment ID: 638] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback.

#021

Posted by **AntonGudiswitz** on **12/15/2024** at **10:41pm** [Comment ID: 751] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

Why are there minimum parking requirements? These requirements drive up the costs of construction and don't have any real economic justification. Can we please accept modern best-practices and move on from 1960s era rules?

#022

Posted by **Laurel David** on **01/24/2025** at **3:20pm** [Comment ID: 972] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

this seems really high for pre-k and primary education where only the staff have cars.

Reply by **SiteAdmin** on **01/28/2025** at **5:04pm** [Comment ID: 991] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks for your feedback.

Use	Parking Zones			
	Zone 1 (PZ1)	Zone 2 (PZ2)	Zone 3 (PZ3)	Zone 4 (PZ4)
Parks and Open Space				
All parks and open space uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: -- Max: --	Min: -- Max: --
Utility				
All utility uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: -- Max: --	Min: -- Max: --
Wireless Telecommunication				
All wireless telecommunication uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: -- Max: --	Min: -- Max: --
Commercial Uses				
Adult Businesses				
All adult business uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Agriculture				
All agriculture uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: -- Max: --	Min: -- Max: --
Entertainment and Recreation				
Lounge or night club	Min: -- Max: 1 per 400 SF	Min: 1 per 200 SF Max: 1 per 400 SF	Min: 1 per 200 SF Max: --	Min: 1 per 100 SF Max: --
Party house	Min: -- Max: 1 per 400 SF	Min: 1 per 200 SF Max: 1 per 400 SF	Min: 1 per 200 SF Max: --	Min: 1 per 100 SF Max: --
All other entertainment and recreation uses	Min: -- Max: 1 per 650 SF	Min: -- Max: 1 per 650 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Food and Beverage				
General food and beverage	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 400 SF Max: --	Min: 1 per 200 SF Max: --
Bakery	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Bar	Min: -- Max: 1 per 400 SF	Min: 1 per 200 SF Max: 1 per 400 SF	Min: 1 per 200 SF Max: --	Min: 1 per 100 SF Max: --
Catering establishment	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 500 SF Max: --	Min: 1 per 250 SF Max: --
Delivery-based commercial kitchen	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 500 SF Max: --	Min: 1 per 250 SF Max: --
Microbrewery, microdistillery, or winery	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --

#023

Posted by **Patrick Ford** on **02/27/2025** at **1:04pm** [Comment ID: 1106] - [Link](#)

Question

Agree: 0, Disagree: 0

Are the min and max numbers flipped? 1 per 200 min on 1200sf establishment would yield 6 slots minimum and 1 per 400 would yield 3 spots maximum.

Reply by **SiteAdmin** on **02/28/2025** at **7:41am** [Comment ID: 1109] - [Link](#)

Answer

Agree: 0, Disagree: 0

Yes. They are. THANK YOU.

#024

Posted by **AntonGudiswitz** on **12/15/2024** at **10:43pm** [Comment ID: 752] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

Why do bars have obscene min parking requirements? Are you trying to encourage drunk driving?

Reply by **SiteAdmin** on **12/16/2024** at **2:02pm** [Comment ID: 762] - [Link](#)

Answer

Agree: 0, Disagree: 0

The existing City Council established this policy in its recent updates to the Beltline Overlay. As you may recall, the initial draft of the Beltline Overlay updates included no requirement for these uses and was amended as shown here.

SITE ACCESS AND PARKING

Use	Parking Zones			
	Zone 1 (PZ1)	Zone 2 (PZ2)	Zone 3 (PZ3)	Zone 4 (PZ4)
Lodging				
Short-term rental	Min: -- Max: 2 per lodging unit	Min: -- Max: 2 per lodging unit	Min: 0.5 per dwelling unit Max: --	Min: 1 per dwelling unit Max: --
All other lodging uses	Min: -- Max: 1 per lodging unit	Min: -- Max: 1 per lodging unit	Min: 0.5 per lodging unit Max: --	Min: 1 per lodging unit Max: --
Medical				
All medical uses	Min: -- Max: 1 per 400 SF	Min: -- ⁰²⁵ Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Office				
All office uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF ⁰²⁶ Max: --
Parking				
All parking uses	Min: -- Max: --	Min: -- Max: --	Min: -- Max: --	Min: -- Max: --
Personal Service				
All personal service uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Retail				
Grocery store	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 1,000 SF Max: --	Min: 1 per 500 SF Max: --
All other retail uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 600 SF Max: --	Min: 1 per 300 SF Max: --
Transportation				
All transportation uses	Min: -- Max: --	Min: -- Max: --	Min: -- Max: --	Min: -- Max: --
Vehicle Sale and Service				
All vehicle sale and service uses	Min: -- Max: 1 per 400 SF	Min: -- Max: 1 per 400 SF	Min: 1 per 1,000 SF Max: --	Min: 1 per 500 SF Max: --
Wholesale Trade				
All wholesale trade uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: 1 per 1,000 SF Max: --	Min: 1 per 500 SF Max: --
Industrial Uses				
Industrial and Manufacturing				
All industrial and manufacturing uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: 1 per 2,000 SF Max: --	Min: 1 per 1,000 SF Max: --

#025

Posted by **phiae01** on **12/30/2024** at **4:02pm** [Comment ID: 799] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

This seems odd to have maximum parking for medical facilities... Most folks visit medical facilities when they are unwell - plus the need for employee parking. Unwell folks are not likely to use transit. Perhaps remove the max for medical category.

Reply by **SiteAdmin** on **01/06/2025** at **1:03pm** [Comment ID: 844] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback. We will take this into consideration as we revise Module II.

#026

Posted by **jessicahilltroutman** on **12/10/2024** at **8:25am** [Comment ID: 714] - [Link](#)

Agree: 0, Disagree: 0

Consider providing a maximum of 1 per 600 in Zone 4 for office use and providing a reduced parking requirement for office uses over a certain threshold size such as 50,000sf.

Reply by **SiteAdmin** on **12/10/2024** at **10:20am** [Comment ID: 717] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks for your feedback!

Use	Parking Zones			
	Zone 1 (PZ1)	Zone 2 (PZ2)	Zone 3 (PZ3)	Zone 4 (PZ4)
Warehouse and Distribution				
All warehouse and distribution uses	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: 1 per 2,000 SF Max: --	Min: 1 per 1,000 SF Max: --
Waste-Related Service				
All waste-related service	Min: -- Max: 1 per 500 SF	Min: -- Max: 1 per 500 SF	Min: 1 per 2,000 SF Max: --	Min: 1 per 1,000 SF Max: --

KEY: -- = no requirement SF = square feet of gross floor area

2. Location of Parking

- Where on-site parking is provided, parking lots and parking structures must adhere to the standards in **Sec. XX. Vehicle Parking Design**.
- Where the Director determines on-street parking is abutting the lot, on-street parking spaces may count toward the minimum parking requirements.
- Tandem parking is permitted for residential uses provided that both parking spaces in tandem are assigned to the same dwelling unit.

3. Electric Vehicle Charging

- Where a parking lot or structure with 6 or more spaces is provided, 10% of the parking spaces must be equipped with conduit and electrical capacity to accommodate future installation of electrical vehicle charging equipment. 028
- Where electrical vehicle charging equipment is provided, each space may count as 2 spaces toward the minimum parking requirement. Spaces must be marked and reserved for electric vehicle parking only. 030

4. Transportation Management Plan (TMP) 029

All nonresidential development greater than 25,000 square feet in floor area must submit a transportation management plan (TMP) to the City, including commitment of Transportation Management Association (TMA) support.

5. Alternative Parking Allowance 027

A reduction in minimum parking required may be authorized by the Director through one or more of the following alternative parking strategies.

a. Senior Housing

A residential use specifically for seniors 55 and older or persons with disabilities may reduce the minimum required parking to 0.25 spaces per dwelling unit or bedroom. 031

b. Affordable Housing

A residential use providing development that includes affordable housing meeting the

#027

Posted by **jessicahilltroutman** on **12/10/2024** at **8:32am** [Comment ID: 715] - [Link](#)

Agree: 0, Disagree: 0

Consider adding an accommodation for reduced parking to the extent off site parking spaces are provided in a location exceeding 600' away to the extent a shuttle is provided. This would help with interim parking deficiencies during construction.

Reply by **SiteAdmin** on **12/10/2024** at **10:03am** [Comment ID: 716] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback.

#028

Posted by **Michael Barnett** on **03/31/2025** at **8:49pm** [Comment ID: 1162] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

If the requirement is 10% why not start at 10 parking spaces? Or are we rounding up so that anything more than 5 would have at least one EV charging station?

Reply by **SiteAdmin** on **04/02/2025** at **2:11pm** [Comment ID: 1184] - [Link](#)

Answer

Agree: 0, Disagree: 0

Good point. I think this is current code, but we will confirm this.

#029

Posted by **haney8604** on **12/17/2024** at **4:34pm** [Comment ID: 763] - [Link](#)

Question

Agree: 0, Disagree: 0

Where can the required contents of the TMP be found?

Reply by **SiteAdmin** on **12/18/2024** at **1:49pm** [Comment ID: 789] - [Link](#)

Answer

Agree: 0, Disagree: 0

This is not currently codified in zoning. We will reach out to the appropriate parties to see if they have standards.

#030

Posted by **haney8604** on **12/17/2024** at **4:34pm** [Comment ID: 764] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

As parking requirements drop it might be prudent to increase this percentage, (or change the requirement to a number per building/parking lot/area rather than a %)

Reply by **SiteAdmin** on **12/18/2024** at **1:56pm** [Comment ID: 791] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks for the suggestion.

#031

Posted by **AntonGudiswitz** on **12/15/2024** at **10:47pm** [Comment ID: 753] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Why not allow zero min parking with approval? Why not always allow zero parking?

Reply by **phiae01** on **12/30/2024** at **4:05pm** [Comment ID: 800] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Employees. Be careful removing or reducing parking because you may need a certain number of care givers around the clock.

SITE ACCESS AND PARKING

requirements of **Sec. XX. Affordable Housing** may reduce the minimum required parking to 0.25 spaces per dwelling unit.

c. Mixed Use

The Director may grant a reduction of the minimum parking requirement in a mixed use development provided one of the followings standards is met:

- i. The development includes uses with different peak hour parking demands or operating hours; or
- ii. The development includes commercial uses that serve on-site residents or employees.

d. Shared Parking

Shared off-site parking facilities may be used to meet the minimum parking requirement for multiple uses that are located near one another with different peak demands or operating hours provided that the following standards are met:

- i. The shared parking facility is within 600 feet walking distance from the primary entry of each participating use;
- ii. Participating uses have mutually exclusive periods of peak parking demand; and
- iii. The applicant provides a written agreement between the property owners, to be recorded with City, guaranteeing the use of the shared parking area.

e. Proximity to Transit

032

033

When a lot is within 1/2 mile walking distance of a high-capacity transit station lot or entry, the lot may apply the parking requirements of Parking Zone 1 (PZ1).

D. Measurement

1. Required parking spaces are calculated as the sum of all required spaces for each principal use on a lot, accounting for all floor area on a site.
2. When the application of these regulations results in a fraction, fractions of 1/2 or more are counted as 1 space.
3. Walking distance is measured according to **Sec. XX. Walking Distance**.

E. Relief

1. A change of up to 10% from vehicle parking standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

#032

Posted by **KUA** on **12/05/2024** at **3:56pm** [Comment ID: 621] - [Link](#)

Question

Agree: 0, Disagree: 0

where is the elimination of parking requirements for buildings built before 1965??????????????

Reply by **SiteAdmin** on **12/06/2024** at **1:41pm** [Comment ID: 635] - [Link](#)

Answer

Agree: 0, Disagree: 0

This was moved to Sec. 8.3.4.B.2.

#033

Posted by **KUA** on **12/05/2024** at **3:54pm** [Comment ID: 619] - [Link](#)

Question

Agree: 0, Disagree: 0

Where is the ability to count on street spaces? Ideally these would count as two off street spaces per each on street because of the much greater turnover.

Reply by **SiteAdmin** on **12/06/2024** at **1:44pm** [Comment ID: 637] - [Link](#)

Answer

Agree: 0, Disagree: 0

It is in 2.B (Location of Parking) above.

Sec. 8.3.5. Vehicle Parking Design

A. Intent

To ensure parking lots are designed to create safe, comfortable, and attractive environments for users and pedestrians, while also mitigating negative environmental effects through landscaping and surface design.

B. Applicability

All parking lots or parking structures, when parking is provided, must meet the vehicle parking design standards.

C. Standards

1. General

- a. All portions of a parking lot or parking structure must be accessible by vehicles to all other portions of the parking lot or structure without requiring the use of a street, except for an alley.
- b. Each parking space must be located so that no vehicle is required to back onto any sidewalk or street, except for an alley, or sidewalk to leave the parking space.
- c. Full and permanent parking space delineation is required. Delineation may include striping, wheel stops, curbing, or other similar permanent material which can clearly define and delineate parking spaces. Full parking space delineation means clear markings for all 3 sides of a space.
- d. All parking lots must meet the design and landscape standards of *City of Atlanta Chapter 158, Article II - Tree Protection*.

2. Spaces and Drive Aisles

- a. Parking spaces and drive aisles must meet the following minimum dimensions:

Angle	Standard Space		Compact Space		One-Way Drive		Two-Way Drive	
	Width	Length	Width	Length	Drive Aisle	Bay Width	Drive Aisle	Bay Width
0°	034	20'	8'	18'	10'	18'	20'	36'
60°	9'	20'	8'	20'	14'	54'	--	--
90°	9'	18'	8'	18'	--	--	22'	58'

- b. Compact parking spaces may be used in place of a standard size space if the following requirements are met:
 - i. The total number of compact car spaces cannot exceed 15% of the total number of required parking spaces. 035
 - ii. All compact parking spaces must be clearly and visibly designated and striped.

#034

Posted by **KUA** on **12/05/2024** at **3:54pm** [Comment ID: 620] - [Link](#)

Question

Agree: 0, Disagree: 0

why are we increasing this from 7.5'?

Reply by **SiteAdmin** on **12/06/2024** at **1:38pm** [Comment ID: 634] - [Link](#)

Answer

Agree: 0, Disagree: 0

This is an error and will be fixed in the next draft.

#035

Posted by **AntonGudiswitz** on **12/15/2024** at **10:56pm** [Comment ID: 754] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

More compact spaces should be allowed. Not every car is a land yacht, we shouldn't have to cater to them.

3. Lighting

Parking lot and parking structure lighting must meet the standards in [Div. XX. Lighting](#).

4. Electric Vehicle Charging

- a. Electric vehicle charging equipment, including pedestals, bollards, or cables cannot encroach into drive aisles or pedestrian walkways.
- b. Electric vehicle charging equipment cannot contain advertising signage that is visible from a public right-of-way.
- c. Transformers and other utility equipment associated with electric vehicle chargers must be screened from the public right-of-way according to [Sec. XX. Frontage Screens](#).

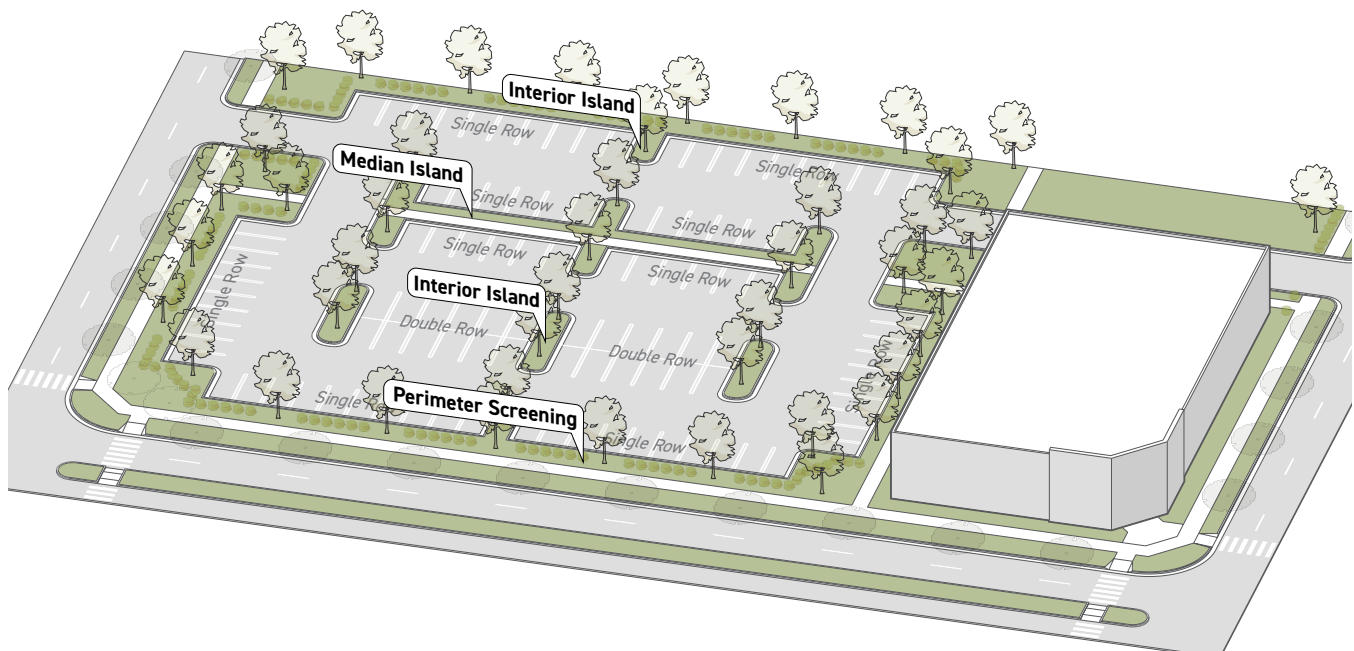
5. Parking Lot Landscaping

a. Perimeter Landscaping

- i. A transition screen may be required between the vehicle use area and a common lot line, see [Sec. XX. Transition Screens](#) for requirements.
- ii. A frontage screen may be required between the vehicle use area and street lot line, see [Sec. XX. Frontage Screens](#) for requirements.

b. Median Islands 036

- i. A landscape median island must be provided between every 4 single parking rows. Intervals may be expanded to preserve existing trees.
- ii. A median island must be at least 6 feet wide. A median island with a pedestrian walkway must be a minimum of 12 feet wide.
- iii. A median island must contain shrubs or ground cover plantings for full coverage of the island at the time of maturity.



#036

Posted by **haney8604** on **12/17/2024** at **4:50pm** [Comment ID: 767] - [Link](#)

Question

Agree: 1, Disagree: 0

Are there any built-in provisions for what should happen when median requirements limit the total number of spaces such that the site cannot meeting minimum parking requirements? Thinking of trying to save the city's time in reviewing such questions.

Reply by **SiteAdmin** on **12/18/2024** at **1:41pm** [Comment ID: 786] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks. We will look into adding this as relief.

6. Parking Structure Screening

Where a parking structure is visible from a public right-of-way or residential property, it must be screened with a permanent structure that meets the following standards:

- a. The screen cannot have an opacity less than 60 percent;
- b. The screening material must be similar in character to the architectural character of building. Chain link fencing is prohibited;
- c. Openings in the screen must be 4 inches or less in one dimension, except for openings providing pedestrian or vehicular access;
- d. Screen openings accommodating vehicle access can be no wider than the maximum allowed driveway width allowed by the zoning district; and
- e. Screen openings accommodating pedestrian access can be no wider than 8 feet for each pedestrian passageway. Where an opening accommodates both pedestrians and vehicles, the maximum allowable opening is 8 feet greater than the maximum allowed driveway width.

D. Measurement

1. Parking Angle

The angle measured from the long edge of a parking space to the drive aisle.

2. Parking Space Width

The distance measured between opposite sides of the shortest dimension of a parking space.

3. Parking Space Length

The distance measured between opposite ends of the longest dimension of a parking space.

4. Bay Width

The distance measured between 2 rows of parking spaces and the drive aisle in-between, or for a single row, the distance between a row of parking spaces and the end of the drive aisle.

5. Drive Aisle

The distance measured from the edge of one row of parking to the edge of the opposite row of parking. If there is only one row of parking spaces, then the paved surface for a vehicle drive on must meet the minimum drive aisle width.

#037

Posted by **Laurel David** on **01/24/2025** at **3:29pm** [Comment ID: 973] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

"Screened with a permanent structure" is confusing. It sounds like a fence, but even an 8-foot fence won't screen a parking deck. It is not clear if screening that is part of the facade's architectural material is allowed.

Reply by **SiteAdmin** on **01/28/2025** at **5:06pm** [Comment ID: 992] - [Link](#)

Answer

Agree: 0, Disagree: 0

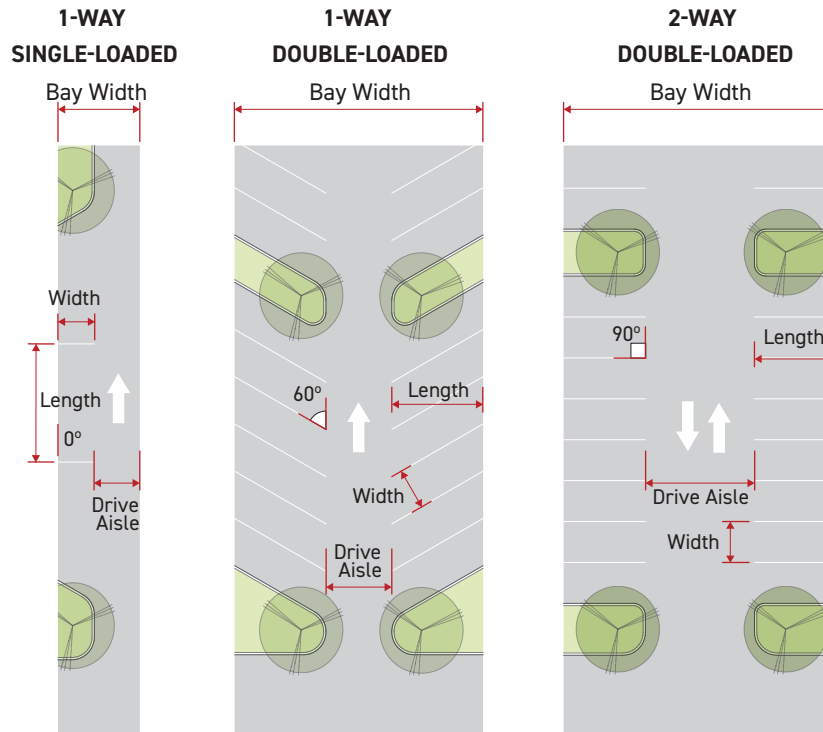
This will be updated. Thanks/

#038

Posted by **Selena Xayavong** on **12/05/2024** at **12:38pm** [Comment ID: 599] - [Link](#)

Agree: 0, Disagree: 0

"paved surface for vehicle to drive on"-could this be shortened/simplified?



6. Interior and Median Island Width

Interior and median island width is measured as the narrowest horizontal dimension from one edge of an island to the opposite edge.

E. Relief

1. A change of up to 10% from vehicle parking design standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.3.6. Vehicle Use Areas

A. Intent

To ensure vehicle use areas are designed in a manner that does not detract from the safety, comfort, or enjoyment of users of neighboring lots or the public realm.

B. Applicability

1. All portions of a lot designed and intended for use by vehicles must comply with the following standards, including areas used for circulation, maneuvering, loading, staging, queuing, service, and areas to be used for the sale or storage of vehicles.
2. Parking lots and structures are excluded from the vehicle use area requirements; see **Sec. XX. Vehicle Parking Design** for parking lot and structure standards.
3. Loading standards apply to any lot that includes on-site loading areas.
4. Stacking space standards apply to any lot that includes an on-site drive-thru or control gate.

C. Standards

1. General

- a. Vehicle use area surfacing must meet the standards in **Sec. XX. Vehicle Parking Design**.
- b. A transition screen may be required between the vehicle use area and a common lot line, see **Sec. XX. Transition Screens** for requirements.
- c. A frontage screen may be required between the vehicle use area and street lot line, See **Sec. XX. Frontage Screens** for requirements.
- d. Vehicle use area lighting must meet the standards in **Div. XX. Lighting**.

2. Loading

- a. Adequate space must be made available on-site for the unloading and loading of goods, materials, items, or stock for delivery and shipping. Unless otherwise modified by the Director, off-street loading spaces must be provided as follows:

Use	Loading Spaces (min)
Residential Uses	
Up to 50 dwelling units	None
51 to 200 dwelling units	1
Above 200 dwelling units	2
All other uses	
Up to 15,000 SF of gross floor area	None
15,001 - 250,000 SF of gross floor area	1
Above 250,000 SF of gross floor area	2

#039

Posted by **KUA** on **12/05/2024** at **3:57pm** [Comment ID: 622] - [Link](#)

Question

Agree: 0, Disagree: 0

We have historically been able to show loading spaces in drive aisles for projects and just focus on time of day for deliveries. This rewrite makes this illegal. Why are we requiring loading spaces at all?

Reply by **SiteAdmin** on **12/06/2024** at **1:15pm** [Comment ID: 632] - [Link](#)

Answer

Agree: 0, Disagree: 0

This is very similar (if not more flexible than) the current language, but we will clarify this current allowance.

#040

Posted by **phiae01** on **12/30/2024** at **4:10pm** [Comment ID: 801] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Seems that every residential building should have at least some capability for loading - people will have to move in and move out, the have large things delivered...

If you can use some other dual purpose space for this, then it should be stated clearly.

Reply by **SiteAdmin** on **01/06/2025** at **1:06pm** [Comment ID: 845] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback. We will consider this as we revise Module II.

- b. Loading spaces must be a minimum of 35 feet in length and 12 feet in width.
- c. Where possible, loading spaces must be located to the rear or side of buildings.
- d. Loading and unloading activities cannot encroach on or interfere with sidewalks, multi-modal lanes, driveways, drive aisles, stacking spaces, or parking lots.
- e. Where the Director determines on-street loading space is abutting the lot, on-street loading spaces may count toward the minimum loading requirements.
- f. Buildings built before 1965 are not required to meet minimum loading requirements.

3. Stacking

- a. Unless otherwise expressly modified by the Director, off-street stacking spaces must be provided as follows:

Activity Type	Stacking Spaces (min)
Food and beverage drive-thru	
On-site ordering	8
Off-site ordering (pick-up only)	5
ATM or banking service	3
Pharmacy	3
Any other drive-thru	5

- b. Each stacking space must be a minimum of 20 feet in length and 10 feet in width.
- c. Stacking spaces are not permitted between the building and a street lot line.
- d. Stacking spaces cannot encroach on or interfere with sidewalks, multi-modal lanes, driveways, drive aisles, loading areas, or parking lots.
- e. Where deemed necessary by the Director for traffic movement and safety, stacking spaces must be separated from other internal driveways by raised medians or other fixed barriers.
- f. An “escape lane” or aisle, measuring 10 feet in width, must be provided adjacent to the stacking spaces where deemed appropriate by the Director.

041

D. Measurement

1. Stacking Space Length

Stacking space length is measured parallel to the drive-thru lane from one end to the opposite end of the queuing space.

2. Stacking Space Width

Stacking space width is measured perpendicularly to the queuing space length from one end to the opposite end of the queuing space.

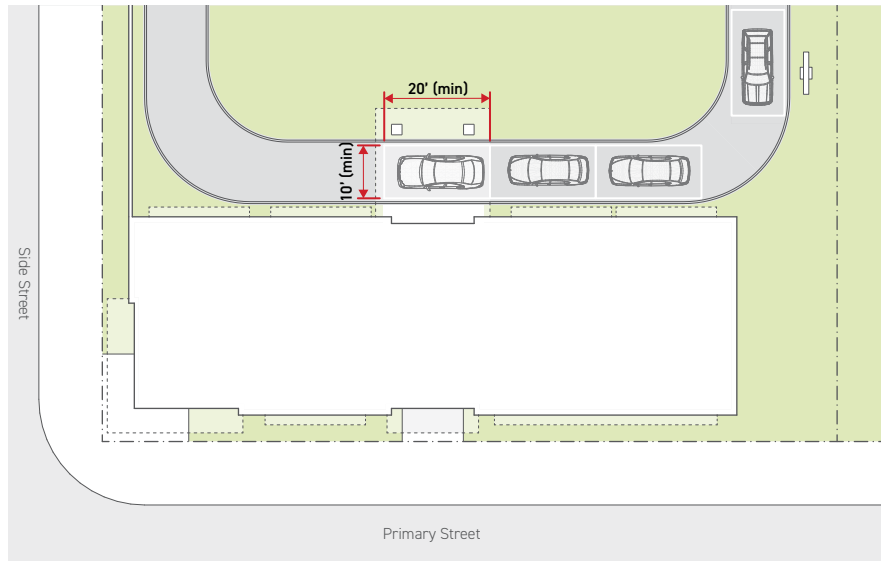
#041

Posted by **Michael Barnett** on **03/31/2025** at **8:57pm** [Comment ID: 1163] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

"lane"



E. Relief

1. A change of up to 10% from vehicle use area standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.3.7. Bicycle Parking

A. Intent

To promote bicycling as an alternative to automobile transportation and help ensure safe, secure, accessible, and convenient storage of bicycles for all users.

B. Applicability

- 1. Bicycle parking standards apply to all lots and the allowed uses in Ch. XX. Use Districts.
- 2. Buildings with up to 4 dwelling units are not required to provide short or long-term bicycle parking.
- 3. Dwelling units with individually accessed private garages are not required to provide long-term bicycle parking.

C. Standards

1. Required Bicycle Parking

Bicycle parking must be provided in accordance with the following table:

Uses	Spaces Required (min)	
	Short-Term Spaces	Long-Term Spaces
Residential		
All residential uses	1 per 5 dwelling units, 2 min	1 per 10 dwelling units, 2 min
Nonresidential		
Office uses	1 per 8,000 SF, 2 min	1 per 8,000 SF, 2 min
All other uses	1 per 4,000 SF, 2 min	None

2. Bicycle Facilities

a. General

- i. Bicycle parking must be directly accessible from a street lot line without the use of an elevator or stairs. 042
- ii. Bicycle parking must consist of bicycle racks that support the bicycle frame at two points.
- iii. Racks must allow for the bicycle frame and at least one wheel to be locked to the rack.
- iv. If bicycles can be locked to each side of the rack, each side can be counted as a required space.
- v. Spacing of bicycle racks must provide clear and maneuverable access.
- vi. Bicycle parking spaces must be located on paved or pervious, durable free surface with a slope no greater than 3 %. Racks must be securely anchored to a permanent surface. 043

#042

Posted by **AntonGudiswitz** on **12/15/2024** at **11:10pm** [Comment ID: 755] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

This standard is much appreciated.

#043

Posted by **KUA** on **12/05/2024** at **3:58pm** [Comment ID: 623] - [Link](#)

Question

Agree: 0, Disagree: 0

Could you clarify what you mean by a dust free surface? Pollen?

Reply by **SiteAdmin** on **12/06/2024** at **1:07pm** [Comment ID: 631] - [Link](#)

Answer

Agree: 0, Disagree: 0

This is term used in the parking and landscape design to specify that the surface doesn't produce dust when used. For example, gravel is NOT a dust-free surface, but brick or stone typically are. It has nothing to do with pollen.

- vii. Bicycle parking must be provided in a well-lit area.
- viii. When bicycle parking is located adjacent to the pedestrian zone, the bicycle parking space cannot obstruct or encroach into the minimum required pedestrian zone.

b. Short-Term Bicycle Parking

- i. Short-term bicycle parking must be publicly accessible and located inside or in an area within 100 feet from the building it serves.
- ii. Short-term bicycle parking may be located within the public right-of-way subject to approval of an encroachment permit for use of the public right-of-way.
- iii. Business operators or property owners who choose to install bicycle parking within the public right-of-way are responsible for maintaining the racks.
- iv. Any site located within 500 feet of a publicly-funded bicycle corral may count up to 2 bicycle parking spaces within the bicycle corral toward their required short-term bicycle parking.

c. Long-Term Bicycle Parking

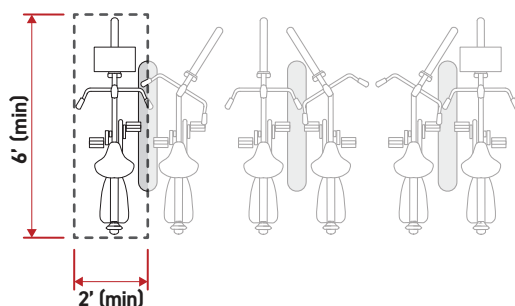
- i. Long-term bicycle parking offers a secure and weather protected place to park bicycles for employees, residents, commuters, and other visitors who generally stay at a site for several hours.
- ii. Long-term bicycle parking is required to be covered and must include use of one of the following:
 - a) A locked room;
 - b) A bicycle locker;
 - c) An area enclosed by a fence with a locked gate;
 - d) An area within view of an attendant or security guard or monitored by a security camera; or
 - e) An area visible from employee work areas.
- iii. Long-term bicycle parking must provide electrical outlets to serve at least 10% of the required number of spaces.
- iv. The bicycle parking area must be located inside or in an area within 200 feet of the building it serves.

d. Showering Facilities

- i. Buildings containing 50,000 square feet of office uses must provide showering facilities and lockers at a ratio of 2 showering facilities for every 50,000 square feet of gross floor area.
- ii. The showering facilities must be available to office tenants and employees.

SITE ACCESS AND PARKING**D. Measurement**

1. When the application of these regulations results in a fraction, fractions of 1/2 or more are counted as 1 space.
2. Distance is measured in walking distance from the nearest point of the bicycle rack to the primary entrance of the use served.
3. Bicycle parking spaces must be a minimum of 2 feet wide and 6 feet long.

**E. Relief**

1. A change of up to 10% from bicycle parking standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

DIVISION 8.4. **TRANSITIONS AND LANDSCAPING**

Sec. 8.4.1. **Transitions**

A. Intent

To improve the compatibility of new development with its surrounding context by managing the scale of development between adjacent lots with different zoning districts.

B. Applicability

1. Transition standards apply to any portion of a lot within 150 feet of a protected district.
2. Transition standards apply along common lot lines where abutting lots have different zoning district designations.
3. Transition standards apply along lot lines that are within 150 feet of a lot with a different zoning district designation.
4. Transition standards do not apply within a lot where a lot has a split zoning designation, where various areas of the same lot have different zoning district designations.

C. Standards

1. General

- a. Where a lot shares a common lot line with a protected district, the contiguous transition standards apply.
- b. Where a lot line is within 150 feet of a protected district but does not share a common lot line, the noncontiguous transition standards apply.
- c. A district is considered protected anywhere a transition is required according to this Section.
- d. Transition standards consist of requirements for yards and building heights near protected districts. Standards for yards include transition screening and building setbacks. Standards for building height include either transitional height planes (THP) or setbacks to push the mass of the building away from protected districts.
- e. Where more than one transition type is listed in the table, the applicant may choose which transition type to provide.
- f. Where a use standard in **Sec. XX. Use Standards** requires screening that conflicts with transition screening requirements, the more intensive requirement must be met.

g. Transitions are required as specified in the following table:

District of Subject Lot	Protected District		
	House-Scale (H-) ⁰⁴⁴	Neighborhood-Scale (N-) ⁰⁴⁶	All Other Districts
House-Scale (H-)			
All districts	--	--	--
Neighborhood-Scale (N-)			
All districts	--	--	--
Urban General (UG-)			
UG-3, UG-5	Type A	Type A or Type B ⁰⁴⁵	⁰⁴⁷
UG-8	Type A	Type A or Type C	--
UG-15, UG-25	Type A	Type A or Type D	--
Urban Core (UC-)			
All districts	Type A	Type A or Type D	--
Workplace Flex (WX-)			
WX-5	Type A	Type A or Type B	--
WX-15	Type A	Type A or Type D	--
Workplace (W-)			
All districts	Type A	Type A or Type D	Type A or Type D
Special			
All districts	Type A	Type A or Type B	--

#044

Posted by **Jennifer Friese** on **12/06/2024** at **6:27pm** [Comment ID: 651] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

N1 (old R4) should be included here - R4 should have the same transitional height plane protection as the other single-family categories R1-R3

Reply by **SiteAdmin** on **12/09/2024** at **11:37am** [Comment ID: 708] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks for your comment.

To be clear, today the "transitions" depend on the zoning of the more intense district, not the zoning of the adjacent "protected" district. See C-3 vs MRC-3 for differences.

#045

Posted by **Jennifer Friese** on **12/06/2024** at **6:25pm** [Comment ID: 650] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

This should only be Type 1 for N1 (which is the old R4). Type B does not provide adequate transition.

Reply by **SiteAdmin** on **12/09/2024** at **11:34am** [Comment ID: 707] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your comment.

Reply by **Michael Barnett** on **03/31/2025** at **9:00pm** [Comment ID: 1164] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

I agree with Ms. Friese. Type B is not appropriate.

#046

Posted by **AntonGudiswitz** on **12/15/2024** at **11:28pm** [Comment ID: 757] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

This zoning rewrite is going to make our code twice as inscrutable. We don't need transitions, just use an intermediate zone in the buffer area if it's a problem somewhere.

#047

Posted by **phiae01** on **12/30/2024** at **4:14pm** [Comment ID: 802] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

UG-3 to other kinds of UG or UC should be specified. Some kind of transition is warranted - especially for the existing RG2. Suggest Type A

Reply by **SiteAdmin** on **01/06/2025** at **1:12pm** [Comment ID: 846] - [Link](#)

Answer

Agree: 0, Disagree: 0

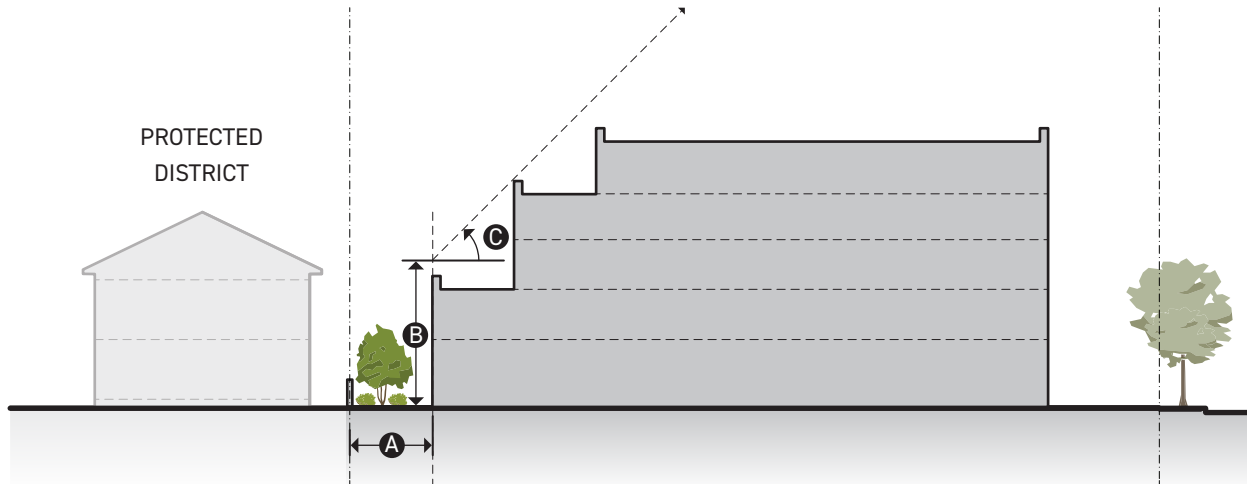
Thank you for your feedback. We will consider this as we revise Module II.

2. Transition Types

TRANSITION TYPE A

CONTIGUOUS

050



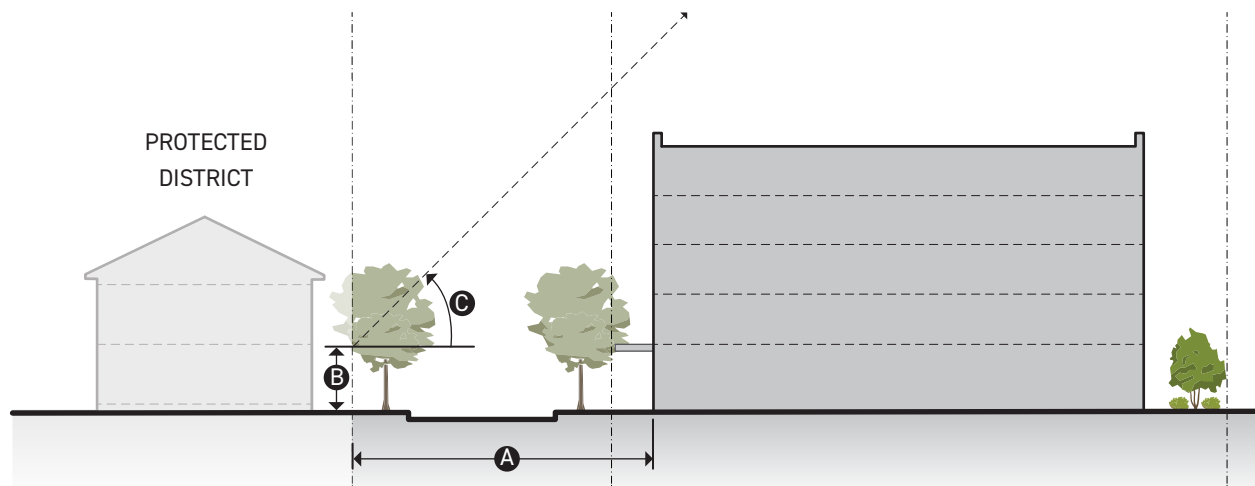
TRANSITION YARD	
Transition screening	Medium
A Building setback (min)	20'

TRANSITION HEIGHT	
B THP height (min)	35'
C THP angle	45°

TRANSITION TYPE A

NONCONTIGUOUS

048



TRANSITION YARD	
Transition screening	None
A Building setback (min)	20'

TRANSITION HEIGHT	
B THP height (min)	15'
C THP angle	45°

#048

Posted by **Jennifer Friese** on **01/06/2025** at **7:32pm** [Comment ID: 849] - [Link](#)

Question

Agree: 0, Disagree: 0

Dumb question, does the existence of an alley between a protected district and bigger district make it non-contiguous?

Reply by **SiteAdmin** on **01/08/2025** at **12:08pm** [Comment ID: 858] - [Link](#)

Answer

Agree: 0, Disagree: 0

Two lots separated by a true "alley" (as opposed to a narrow City street -- see Peace Avenue/Latta Street in O4W) are contiguous.

#049

Posted by **AntonGudiswitz** on **12/15/2024** at **11:22pm** [Comment ID: 756] - [Link](#)

Question

Agree: 0, Disagree: 0

Where does 45 degrees come from? Is there justification? Why can't this be 55 degrees? 60 degrees? It achieves the same aesthetic without being nearly as restrictive.

#050

Posted by **dimnickph** on **12/05/2024** at **1:53pm** [Comment ID: 601] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

I appreciate the simplicity of the calculation but should there be some recognition of the disparity between the two contiguous districts. Should the transitional height plane be different when a R-4 residential zone abuts SPI16 versus when a R-4 district abuts some more moderate height mixed use district?

Reply by **SiteAdmin** on **12/06/2024** at **3:05pm** [Comment ID: 647] - [Link](#)

Question

Agree: 0, Disagree: 0

At this time, we are recommending the same transitions for existing R-4, regardless of the adjacent use. This reflects current practice. Are you suggesting a different transition where R-4 abuts SPI-16 Midtown?

Reply by **dimnickph** on **12/09/2024** at **10:23am** [Comment ID: 688] - [Link](#)

Agree: 0, Disagree: 0

At one of the open houses I attended, there was a table discussion suggesting the concept of a buffer zone whereby the street directly abutting a R1-R5 district would be zoned for use intensity and factors such as height intermediate to an area zoned for higher use intensity. The east side of Peachtree Street between 15th and West Peachtree

Streets would be an example. The new building at 1441 Peachtree provides an example where such a buffer zoning would have been welcomed. The table discussion at the open house was regarding the transition between the R-5 Garden District and Ponce de Leon. The CPD representative seemed to be amicable to the idea. I had hoped the concept survived the idea amalgamation process.

Reply by **SiteAdmin** on **12/09/2024** at **12:17pm** [Comment ID: 710]

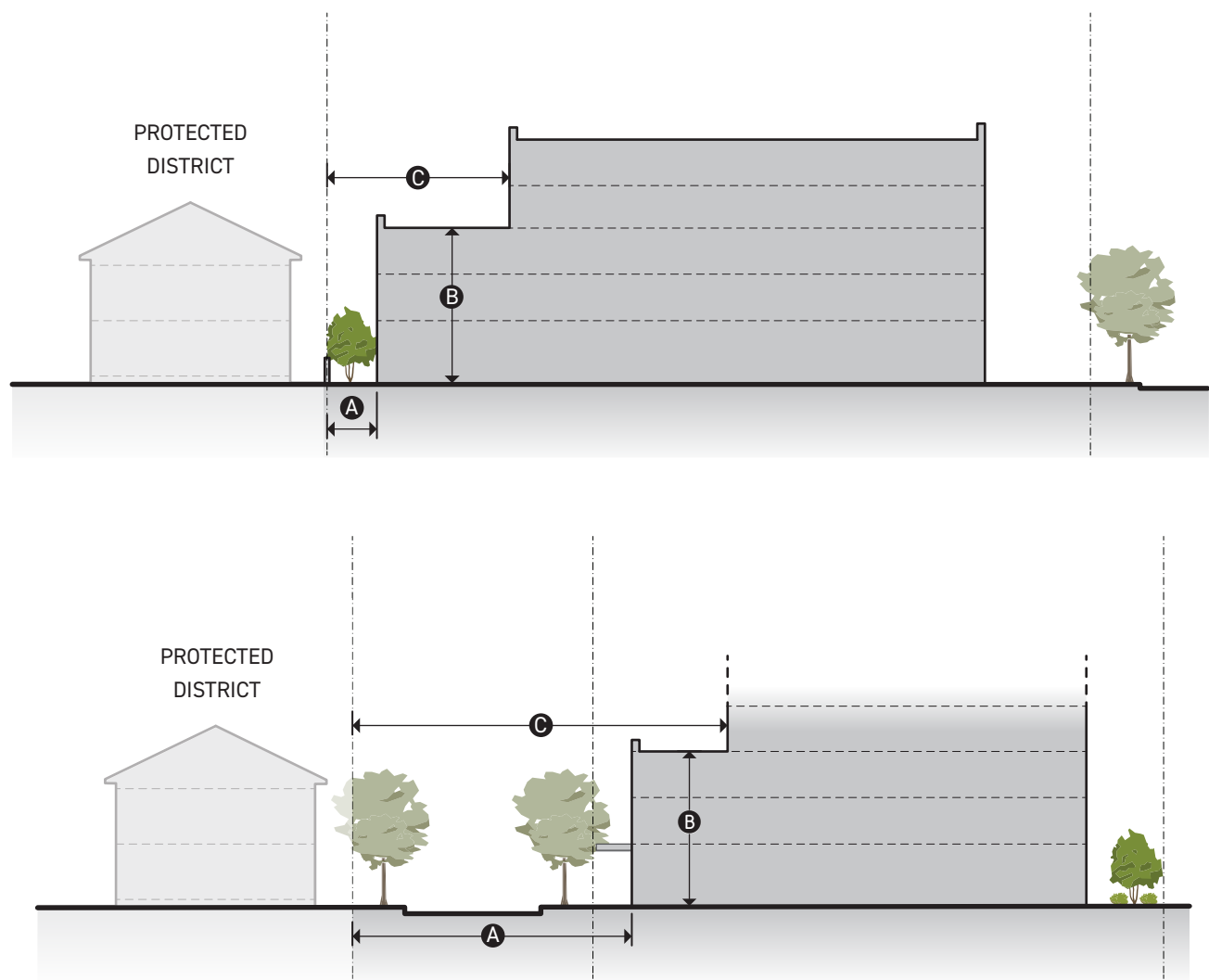
- [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks for your input!

TRANSITION TYPE B



TRANSITION YARD	
Transition screening	Low
A Building setback (min)	
Contiguous	051 1052
Noncontiguous	20'

TRANSITION HEIGHT	
B Height before stepback (max stories/feet)	3 / 40'
C Stepback depth (min)	40'

#051

Posted by **Jennifer Friese** on **12/06/2024** at **6:32pm** [Comment ID: 653] - [Link](#)

Question

Agree: 1, Disagree: 0

Should these be flipped? 20 Contiguous, 10 non-contiguous?

Reply by **SiteAdmin** on **12/09/2024** at **11:30am** [Comment ID: 706] - [Link](#)

Answer

Agree: 0, Disagree: 0

We will look into this and correct any errors in the next draft. Thanks!

#052

Posted by **Jim Winer** on **12/08/2024** at **4:05pm** [Comment ID: 665] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

This appears to be a case of increasing the height and mass in current RG to R boundary areas. It is important to provide a comparative illustrations and share those with the neighborhoods that are effected. Absent doing so and providing similar illustrations of zoning changes the City would not be in a position to claim the neighborhoods were fully informed.

Reply by **SiteAdmin** on **12/09/2024** at **11:13am** [Comment ID: 696] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks. We will clarify these and provide examples at the January 13th Q&A meeting.

Many of these provisions are already found in the Quality-of-Life Zoning Districts (e.g., MR, MRC, LW, etc.).

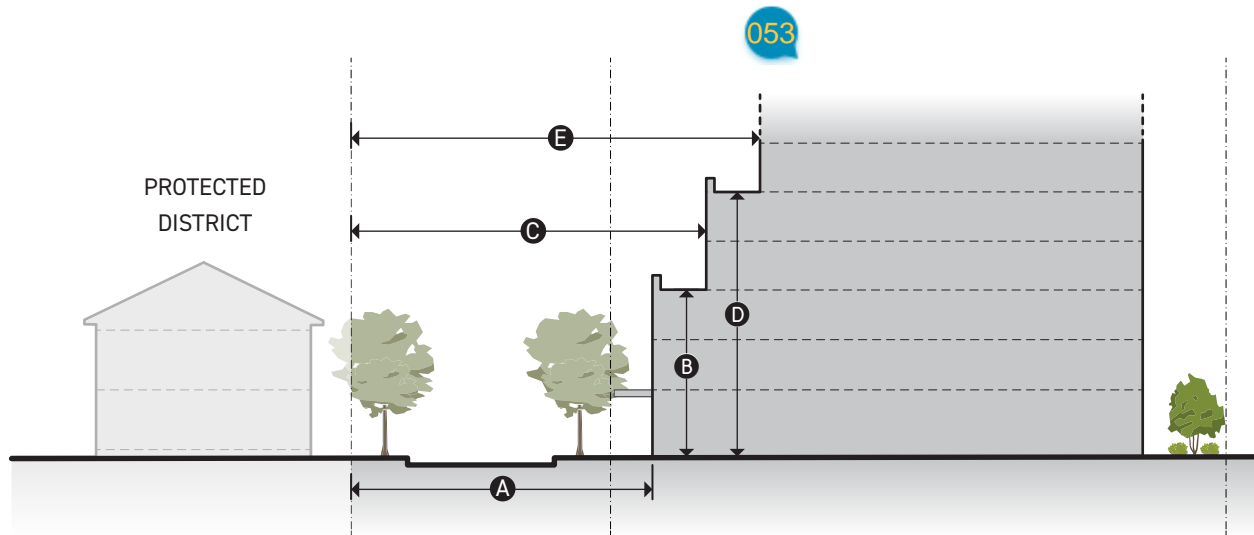
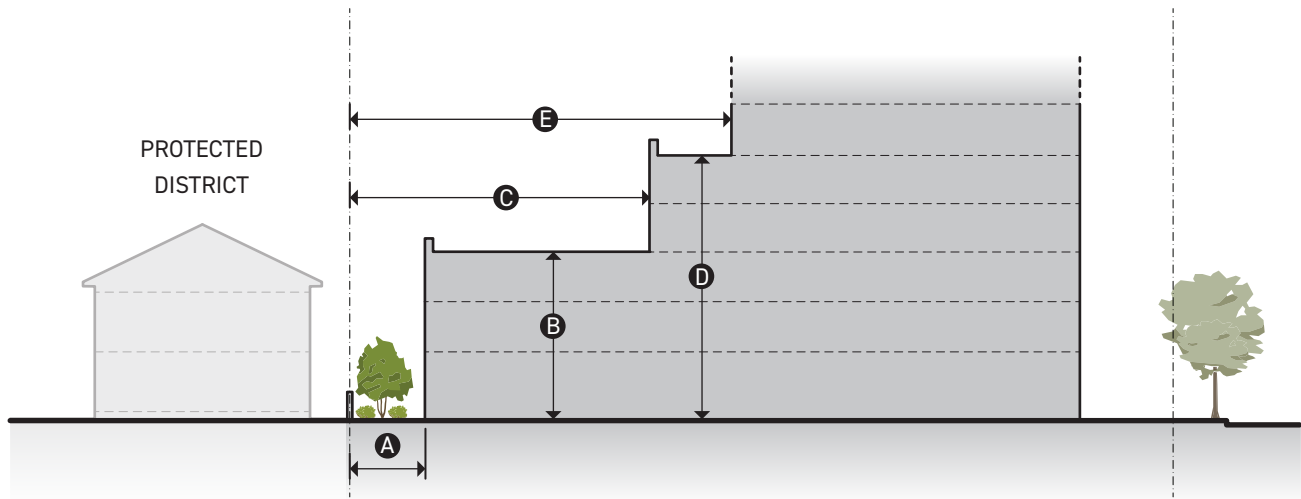
Reply by **phiae01** on **12/30/2024** at **4:16pm** [Comment ID: 803] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Agreed - the removal of the setbacks from RG2 into the new UG-3 is particularly problematic and requires this kind of clarification.

TRANSITION TYPE C



TRANSITION YARD	
Transition screening	Medium
A Building setback (min)	
Contiguous	20'
Noncontiguous	20'

TRANSITION HEIGHT	
B Height before first stepback (max stories/feet)	3 / 40'
C Stepback depth (min)	60'
D Height before second stepback (max stories/feet)	5 / 70'
E Stepback depth (min)	80'

#053

Posted by **AntonGudiswitz** on **12/15/2024** at **11:32pm** [Comment ID: 758] - [Link](#)

Suggestion

Agree: 0, Disagree: -1

How much does it cost a builder to do this ziggurat-style construction? This will absolutely make structures more expensive, and for what? Step-backs don't even look better, it's just one architect's opinion.

Reply by **SiteAdmin** on **12/16/2024** at **1:55pm** [Comment ID: 761] - [Link](#)

Answer

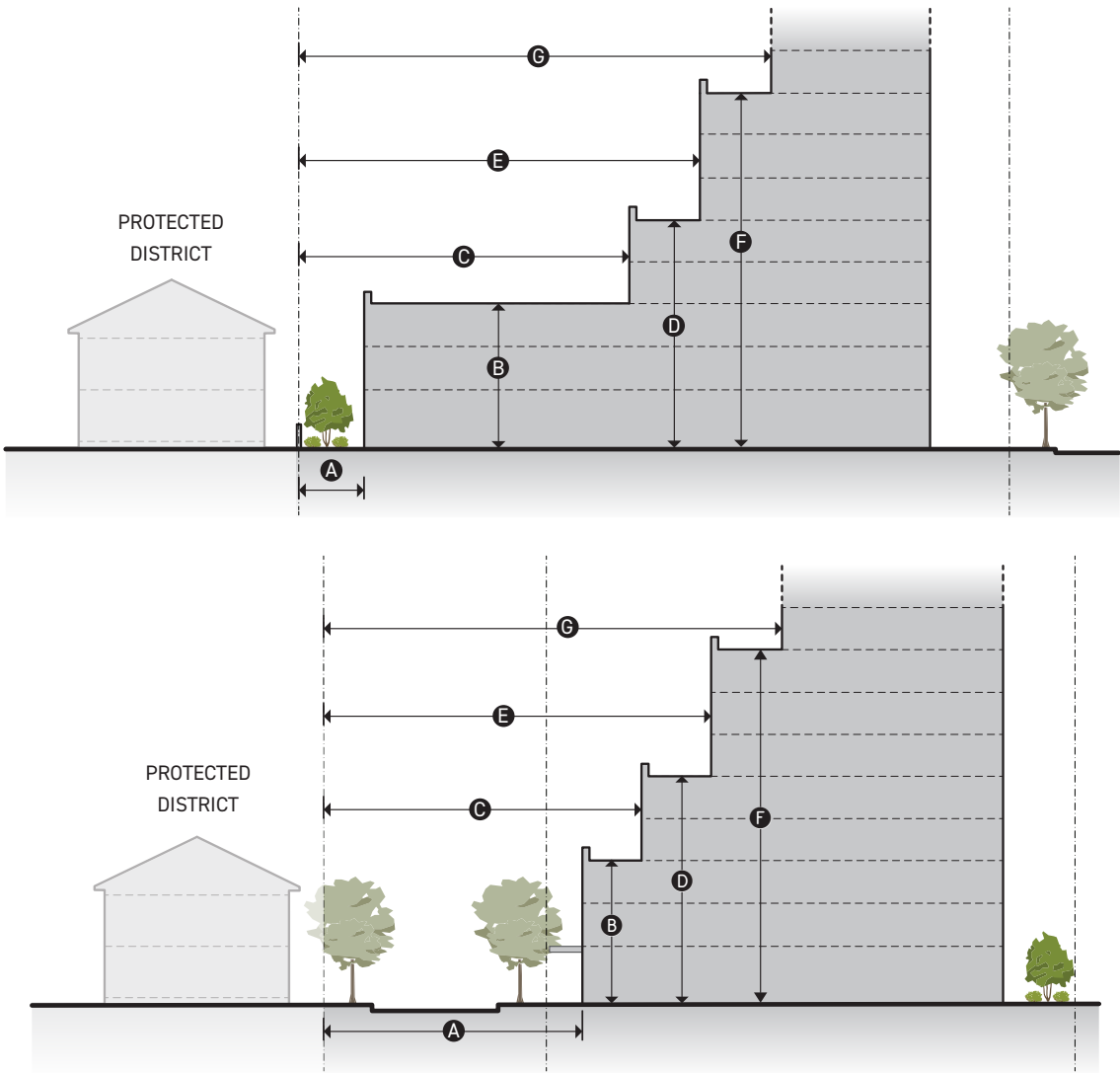
Agree: 0, Disagree: 0

Thank you for your comment.

To-date, we have not received any comments opposing this modified existing provision from the large-scale developers typically subject to it. If this is a concern, we encourage them to share their comments here or by sending an email to atlzoning2@atlantaga.gov.

Lastly, please note that this provision doesn't require stepped construction when the building is set far from the "protected district" Every tall building on Peachtree Street between Buckhead Village and Brookwood Station is subject to this existing provision.

TRANSITION TYPE D



TRANSITION YARD	
Transition screening	High
A Building setback (min)	
Contiguous	20'
Noncontiguous	20'

TRANSITION HEIGHT	
B Height before first stepback (max stories/feet)	3 / 40'
C Stepback depth (min)	80'
D Height before second stepback (max stories/feet)	5 / 70'
E Stepback depth (min)	100'
F Height before third stepback (max stories/feet)	8 / 115'
G Stepback depth (min)	120'

3. Transition Screening

The transition screening standards are determined by the requirements of **Sec. XX. Transition Screens**. Each category of transition screening (low, medium, and high) includes multiple options for meeting the requirement. The option of which transition screen to use within the required category is at the discretion of the applicant.

4. Matching Setback and Height Allowance 056

Instead of meeting the building setback, stepback, or transitional height plane requirements of the transition type, projects may choose to meet the height and building setbacks standards of the protected district within 100 feet of the protected district lot line. Transition screening requirements still apply.

D. Measurement

1. Transitional Height Plane (THP)

- a. For contiguous transitions, the transitional height plane (THP) is measured from average existing grade along the minimum building setback line.
- b. For noncontiguous transitions, the transitional height plane (THP) is measured from average existing grade along the lot line of the protected district.
- c. The THP starts at the height specified by the transition type and continues at an angle into the higher-intensity district.

2. Building Setback

- a. The building setback is measured perpendicular to the protected district lot line. See **Div. XX. Building Setbacks** for measuring building setbacks.
- b. For projects abutting an alley, the building setback is measured from the alley centerline instead of the rear or side lot line. 054

3. Height Before Stepback

Height before stepback is measured as the maximum building height in stories and feet from average existing grade for any portion of a building near a protected district before an additional stepback is required. See **Sec. XX. Average Grade** for measuring average grade and **Sec. XX. Building Height** for measuring building height in stories and feet.

4. Stepback Depth

- a. The step-back depth is measured perpendicularly as a horizontal distance from the protected district lot line to the exterior of the building before an additional stepback is required.
- b. For projects abutting an alley, the stepback depth is measured from the alley centerline instead of the rear or side lot line. 055

#054

Posted by **Jim Winer** on **12/08/2024** at **4:08pm** [Comment ID: 666] - [Link](#)

Agree: 0, Disagree: 0

Excellent. This maintains the current regulation without taking property rights from property owners that abut alleys and legally own to the alley centerlines.

Reply by **SiteAdmin** on **12/09/2024** at **11:10am** [Comment ID: 695] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks.

Nothing in this code will change abutting property owners' alley rights, which are not regulated by zoning.

#055

Posted by **Michael Barnett** on **03/31/2025** at **9:04pm** [Comment ID: 1165] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

I think elsewhere in the code we are stating that the edge of the easement is where set- back requirements begin. Wouldn't it be logical to use that here also?

Reply by **SiteAdmin** on **04/02/2025** at **2:18pm** [Comment ID: 1185] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks. We will look into this.

#056

Posted by **Jennifer Frieese** on **12/06/2024** at **6:41pm** [Comment ID: 654] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

This seems like a good idea to encourage new construction to match existing patterns of less intense adjacent districts.

Reply by **SiteAdmin** on **12/09/2024** at **11:28am** [Comment ID: 705] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks

E. Relief

1. A change of up to 10% from transition standards may be granted in accordance with **Sec. XX. Administrative Adjustment.**
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance.**

Sec. 8.4.2. Transition Screen

A. Intent

To mitigate impacts from uses, activities, or site elements with significant impact on abutting lots.

B. Applicability

Transition screens requirements apply to common lot lines as specified by **Sec. XX. Transitions** and **Div. XX. Use Standards**.

C. Standards

1. General

- a. Required transition screens, including sub-grade elements such as footings or foundations, must be located entirely on-site.
- b. Breaks in required transition screens for pedestrian, bicycle, and vehicular access are allowed, provided the break in the screen is the minimum practical width. Driveways or walkaways must cross at or near a perpendicular angle.
- c. No buildings or structures, except for fences and walls, are allowed in the screening area.
- d. No vehicle use areas are allowed in the screening area, including fire lanes.
- e. To allow for maintenance access, required plantings must be located on the inside of the fence and wall.
- f. All fences and walls must meet the fence and wall design and installation standards, see **Sec. XX. Fences and Walls**.
- g. All required trees and shrubs must meet **Sec. XX. Plants and Plant Material**.

#057

Posted by **Jennifer Frieese** on **12/06/2024** at **6:30pm** [Comment ID: 652] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

This is a great addition to THPs

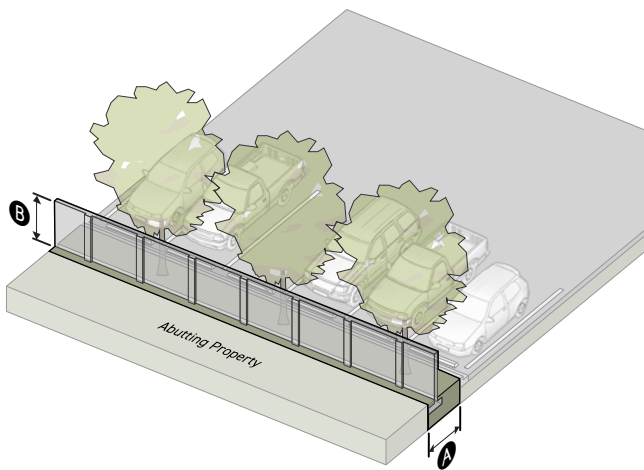
2. Transition Screen Types

a. Low Transition Screens

A required “low” transition screen must meet the standards of one of the following options:

TRANSITION SCREEN - LOW 1

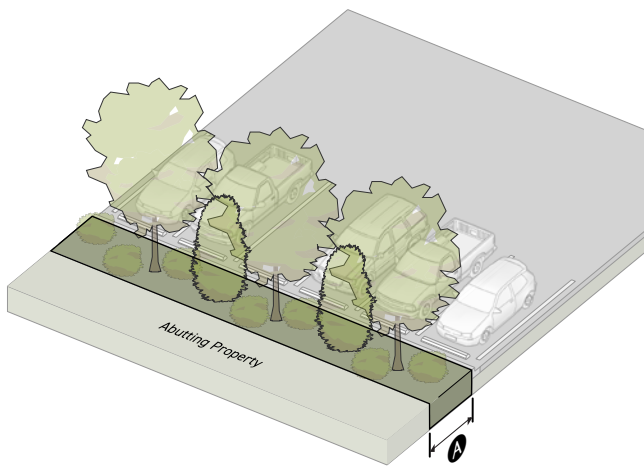
Narrow landscape screening with a wall or fence intended for screening low impact transitions and uses.



SCREENING AREA	
A Screening area depth (min)	6'
Canopy trees (min per 50')	2
FENCES AND WALLS	
B Height (min)	6'
Opacity (min)	90%
Setback from lot line (min)	0'

TRANSITION SCREEN - LOW 2

Moderate depth landscape buffer with no wall or fence intended for screening low impact transitions and uses.



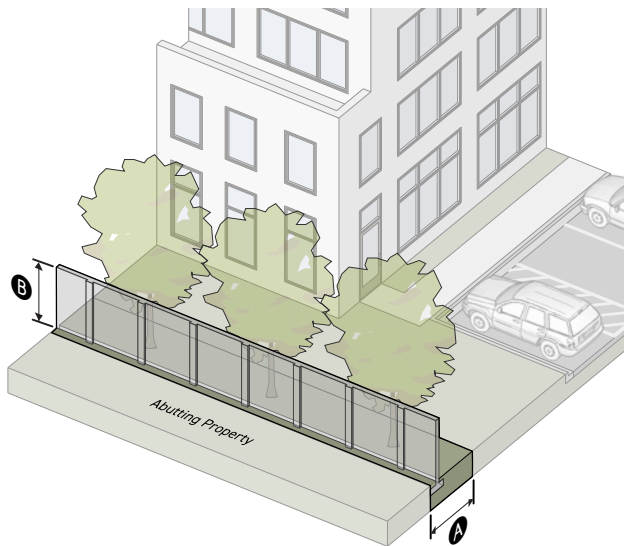
SCREENING AREA	
A Screening area depth (min)	12'
Canopy trees (min per 50')	2
Understory trees (min per 50')	2
Screening plants (min per 50')	8
FENCES AND WALLS	
Not required	

b. Medium Transition Screens

A required “medium” transition screen must meet the standards of one of the following options:

TRANSITION SCREEN - MEDIUM 1

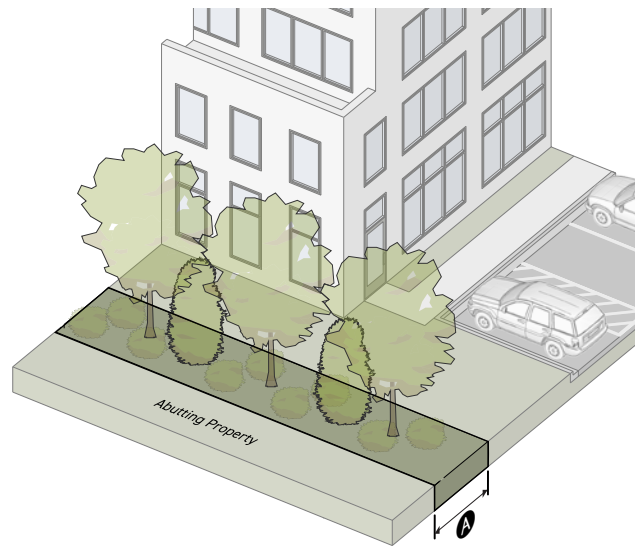
Moderate depth landscape screening with a wall or fence intended for screening medium impact transitions and uses.



SCREENING AREA	
A Screening area depth (min)	10'
Canopy trees (min per 50')	2
FENCES AND WALLS	
B Height (min)	6'
Opacity (min)	100%
Setback from lot line (min)	0'

TRANSITION SCREEN - MEDIUM 2

Deeper landscape buffer with no wall or fence intended for screening medium impact transitions and uses.



SCREENING AREA	
A Screening area depth (min)	20'
Canopy trees (min per 50')	2
Understory trees (min per 50')	2
Screening plants (min per 50')	16
FENCES AND WALLS	
Not required	

c. High Transition Screens

A required “high” transition screen must meet the standards of one of the following options:

TRANSITION SCREEN - HIGH 1

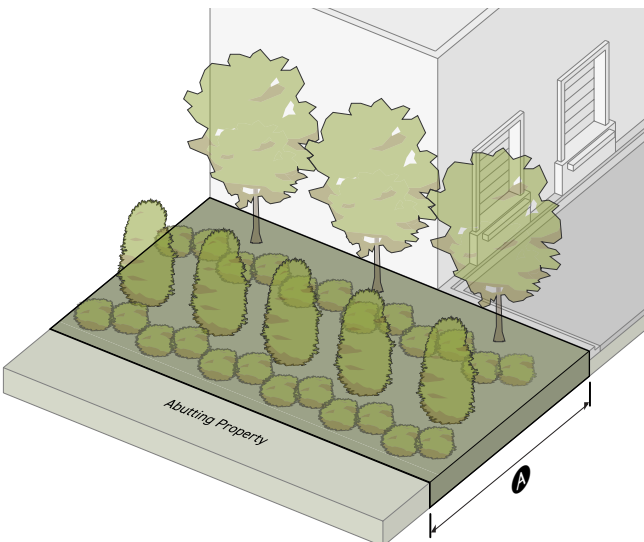
Deeper landscape screening with a wall or fence intended for screening high impact transitions and uses.



SCREENING AREA	
A Screening area depth (min)	15'
Canopy trees (min per 50')	2
Understory trees (min per 50')	2
Shrubs (min per 50')	16
FENCES AND WALLS	
B Height (min)	6'
Opacity (min)	100%
Setback from lot line (min)	0'

TRANSITION SCREEN - HIGH 2

Deep landscape buffer with no wall or fence intended for screening high impact transitions and uses.



SCREENING AREA	
A Screening area depth (min)	30'
Canopy trees (min per 50')	3
Understory trees (min per 50')	5
Screening plants (min per 50')	24
FENCES AND WALLS	
Not required	

D. Measurement

1. Screening Area Depth

Screening area depth is measured perpendicular to the lot line at the narrowest horizontal dimension from one side of the screening area to the opposite side.

2. Planting Frequency

Planting frequency is measured as the total number of trees, shrubs, or plants within any 50-foot segment of screen area.

3. Fence or Wall Height

For measurement of fence or wall height and opacity, see **Sec. XX. Fences and Walls**.

E. Relief

1. A change of up to 10% from transition screen standard may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.4.3. Frontage Screens

A. Intent

To screen the negative impacts of uses on the public realm, promoting visual interest and increasing comfort for uses of the adjacent streetscape.

B. Applicability

Frontage screen requirements apply along street lot lines adjacent to vehicle use, storage, and utility areas.

C. Standards

1. General

- a. Frontage screens are required as specified in the following table:

Frontage Screen Type	
Outdoor Storage	
All outdoor storage areas	High
Vehicle Use Areas	
Parking lots or structures	Low
Drive-thrus	Low
Vehicle storage	Medium
Loading docks or service areas	High
Other vehicle use areas	Low
Utility Areas	
All utility areas	Medium

- b. Required screens, including their sub-grade elements, such as footings or foundations, must be located entirely on-site.
- c. Breaks for pedestrian, bicycle, and vehicular access are allowed, provided the break in the screen is the minimum practical width. Driveways or walkaways must cross at or near a perpendicular angle.
- d. No buildings or structures, except for walls and fences, are allowed in the screening area.
- e. No vehicle use areas are allowed in the screening area, including fire lanes.
- f. All fences and walls must meet the fence and wall design and installation standards, see Sec. XX. Fences and Walls.
- g. All required trees and shrubs must meet Sec. XX. Plants and Plant Material.

#058

Posted by **Laurel David** on **02/03/2025** at **11:37am** [Comment ID: 1015] - [Link](#)

Question

Agree: 0, Disagree: 0

what is an "utility area"? I suggest transformers are exempted from the screening requirement. Power companies have particular standards about what can be placed around their transformers.

Reply by **SiteAdmin** on **02/06/2025** at **3:18pm** [Comment ID: 1040] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for comments. We will address this in the revised Chapter 8 draft.

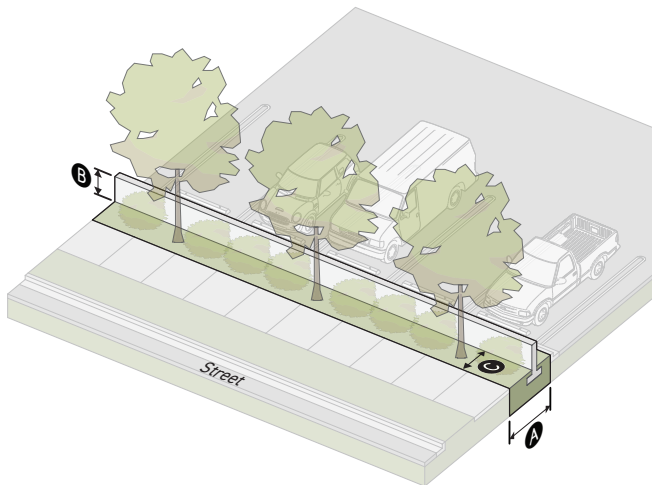
2. Frontage Screen Types

a. Low Frontage Screens

A required “low” frontage screen must meet the standards of one of the following options:

FRONTAGE SCREEN - LOW 1

Narrow landscape screening with a wall or fence intended for screening low impact vehicle use areas.



SCREENING AREA

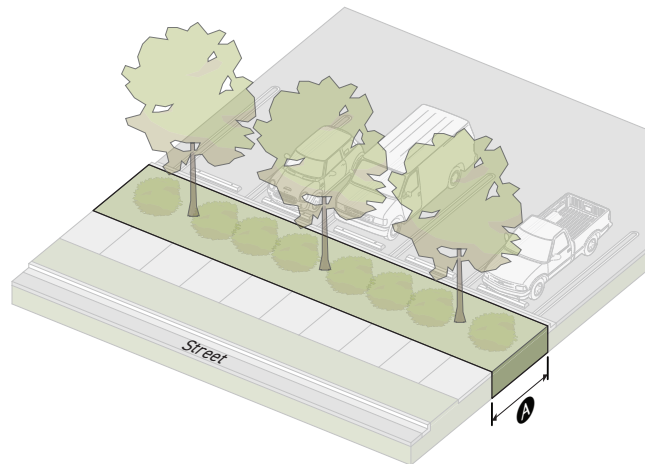
A Screening area depth (min)	8'
Canopy trees (min per 50')	2
Shrubs (min per 50')	8

FENCES AND WALLS

B Height (min)	3.5'
Opacity (min)	50%
C Setback from lot line (min)	6'

FRONTAGE SCREEN - LOW 2

Moderate depth landscape buffer with no wall or fence intended for screening low impact vehicle use areas.



SCREENING AREA

A Screening area depth (min)	12'
Canopy trees (min per 50')	2
Screening plants (min per 50')	12

FENCES AND WALLS

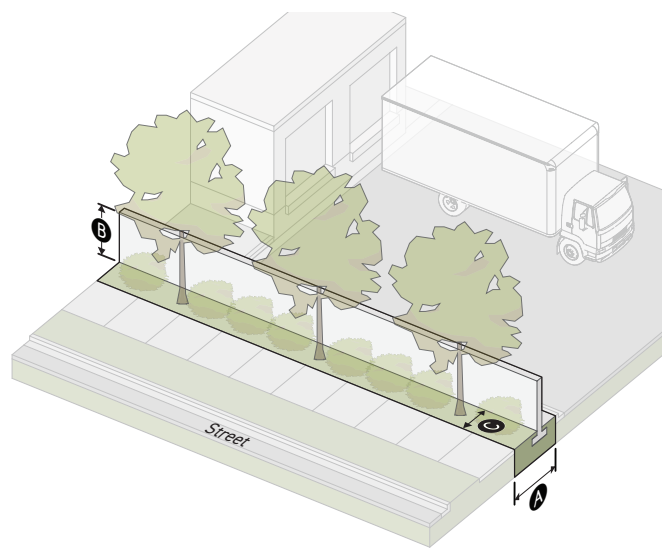
Not required

b. Medium Frontage Screens

A required “medium” frontage screen must meet the standards of one of the following options:

FRONTAGE SCREEN - MEDIUM 1

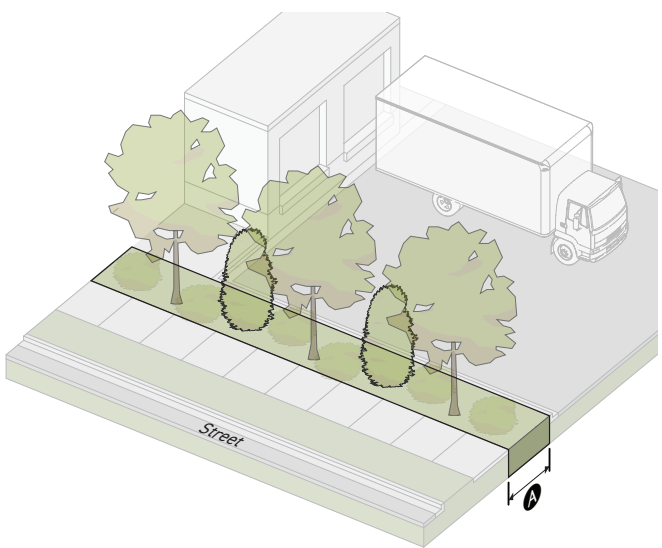
Moderate depth landscape screening with a wall or fence intended for screening moderate impact vehicle use and utility areas.



SCREENING AREA	
A Screening area depth (min)	8'
Canopy trees (min per 50')	2
Shrubs (min per 50')	10
FENCES AND WALLS	
B Height (min)	6'
Opacity (min)	100%
C Setback from lot line (min)	6'

FRONTAGE SCREEN - MEDIUM 2

Deeper landscape buffer with no wall or fence intended for screening moderate impact vehicle use and utility areas.



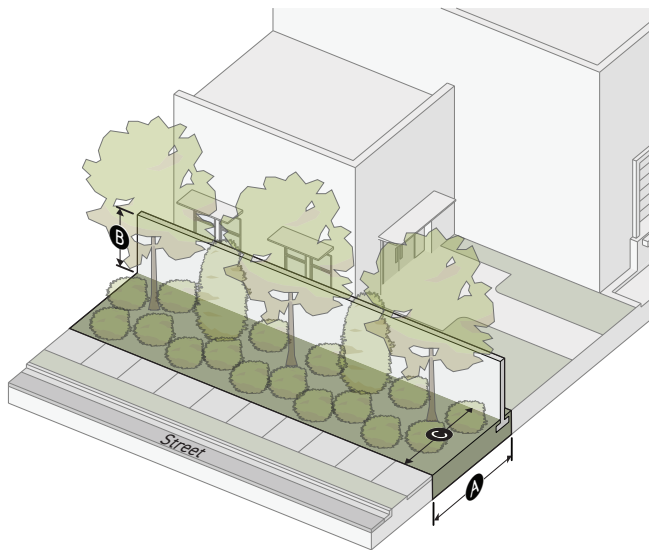
SCREENING AREA	
A Screening area depth (min)	16'
Canopy trees (min per 50')	2
Understory trees (min per 50')	2
Screening plants (min per 50')	16
FENCES AND WALLS	
Not required	

c. High Frontage Screens

A required “high” frontage screen must meet the standards of one of the following options:

FRONTAGE SCREEN - HIGH 1

Deeper landscape screening with a wall or fence intended for screening high impact vehicle use and outdoor storage areas.



SCREENING AREA

A Screening area depth (min)	15'
Canopy trees (min per 50')	2
Shrubs (min per 50')	16

FENCES AND WALLS

B Height (min)	6'
Opacity (min)	100%
C Setback from lot line (min)	6'

FRONTAGE SCREEN - HIGH 2

Deep landscape buffer with no wall or fence intended for screening high impact vehicle use and outdoor storage areas.



SCREENING AREA

A Screening area depth (min)	30'
Canopy trees (min per 50')	3
Understory trees (min per 50')	5
Screening plants (min per 50')	24

FENCES AND WALLS

Not required

D. Measurement

1. Screening Area Depth

Screening area depth is measured perpendicular to the lot line at the narrowest horizontal dimension from one side of the screening area to the opposite side.

2. Planting Frequency

Planting frequency is measured as the total number of trees, shrubs, or plants within any 50-foot segment of screen area.

3. Fence or Wall Height

For measurement of fence or wall height and opacity, see **Sec. XX. Fences and Walls**.

E. Relief

1. A change of up to 10% from frontage screen standard may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.4.4. Site Element Screens

A. Intent

To minimize the visibility and impact of certain site elements, such as mechanical, electrical, or utility equipment and waste receptacles, on surrounding properties and the public realm.

B. Applicability

1. Site element screen requirements apply to all outdoor waste receptacles and all outdoor mechanical or electrical equipment located on the roof of a building, attached to the wall of a building, or on the ground.
2. Site element screen requirements do not apply to the following:
 - a. Electric vehicle chargers;
 - b. Solar panels;
 - c. Phone or cable boxes;
 - d. Electrical meters; and
 - e. Gas meters, where there are 3 or fewer meters in a row.

C. Standards

1. General

All site element screens must meet the fence and wall design and installation standards, see **Sec. XX. Fences and Walls**.

2. Waste Receptacle Screening

- a. Waste receptacles, including in-ground waste receptacles, cannot be located between a street lot line and the building. Waste receptacles must be located in the side or rear yard and meet the encroachment requirements of **Div. XX. Building Setbacks**.
- b. Outdoor waste receptacles with any above ground element must be screened on 3 sides by a fence or wall with a minimum height of 6 feet.
- c. Access gates must be provided on the fourth side and must also be a minimum height of 6 feet.
- d. The fence or wall and access gate must be at least 90% opaque.

#059

Posted by **Laurel David** on **02/03/2025** at **11:42am** [Comment ID: 1016] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

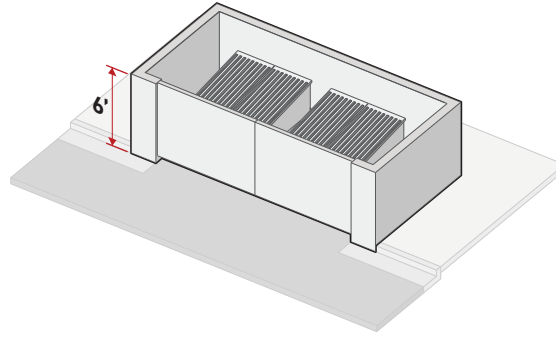
I suggest that herbie curbies are listed as not being a "waste receptacle" so homes are exempt from these requirements. its not clear

Reply by **SiteAdmin** on **02/06/2025** at **3:14pm** [Comment ID: 1039] - [Link](#)

Answer

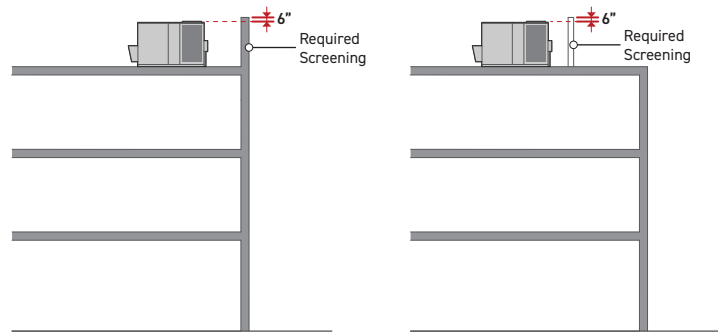
Agree: 0, Disagree: 0

Thanks. We will specify this.



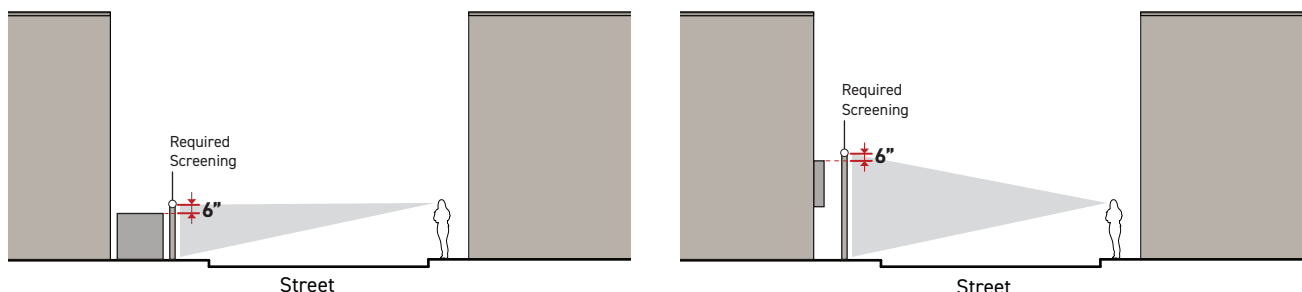
3. Roof-Mounted Equipment Screening

- a. Equipment visible from a street lot line must be screened on the roof edge side by a parapet wall or other type of screen that is at least 6 inches higher than the topmost point of the screened equipment.
- b. The screening must be at least 75% opaque.



4. Ground and Wall-Mounted Equipment Screening

- a. Equipment cannot be located between a street lot line and the building. Equipment must be located in the side or rear yard and meet the encroachment requirements of **Div. XX. Building Setbacks**.
- b. Equipment visible from a street lot line must be fully screened by an opaque fence or wall that is at least 6 inches higher than the topmost point of the screened equipment.
- c. The screening must be at least 75% opaque.



D. Relief

1. A change of up to 10% from site element screen standard may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.4.5. **Fences and Walls**

A. Intent

To facilitate natural surveillance and visual interest along the public realm while ensuring security and privacy for ground story uses in a manner appropriate to the context.

B. Applicability

1. Fence and wall standards apply to all lots.
2. Permitted fence and wall types for each lot are set out in **Ch. XX. Form Districts**.
3. Where a transition, frontage, or site element screening requirement requires a taller fence or wall, the screening requirement supersedes the standards in this Section.

060

#060

Posted by **zperry** on **05/21/2025** at **10:43am** [Comment ID: 1238] - [Link](#)

Agree: 0, Disagree: 0

The current zoning ordinance is very unclear on how fences work with corner lots. Clearing that up would be immensely helpful.

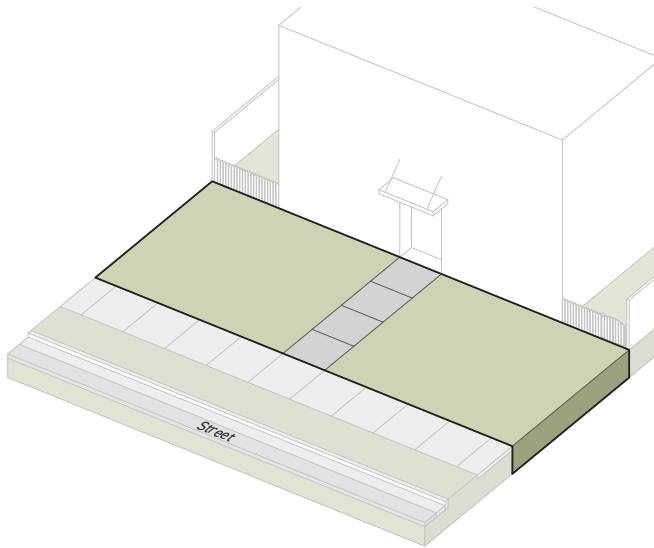
C. Standards

1. Front or Side Street Yard Fence Types

The following fence types are intended for front or side street yards, along street lot lines:

FENCE TYPE A1

Intended for front and side street yards where buildings engage directly with sidewalks to provide natural surveillance and visual interest, especially where ground floor uses are commercial.

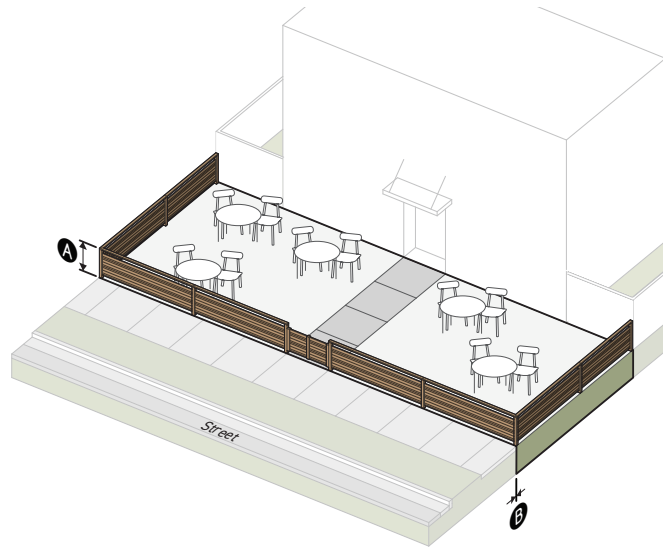


DIMENSIONAL STANDARDS

Height (max)	Not allowed
--------------	-------------

FENCE TYPE A2

Intended for front and side street yards where buildings engage directly with sidewalks and a fence or wall is required for the on-site consumption of alcohol.



DIMENSIONAL STANDARDS

A Height (max)	3.5'
B Setback from lot line (min)	0'
Opacity (max)	50%

#061

Posted by **Laurel David** on **02/03/2025** at **11:50am** [Comment ID: 1018] - [Link](#)

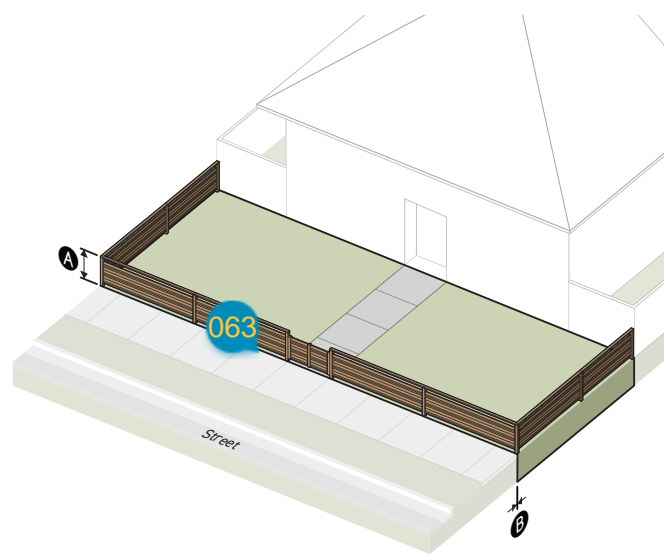
Suggestion

Agree: 0, Disagree: 0

It is not clear whether the standards apply to walls as well. They are all called Fence Type. It also isn't clear if no opacity max is mentioned then that means it can be 100%. Maybe that should be added where applicable

FENCE TYPE A3

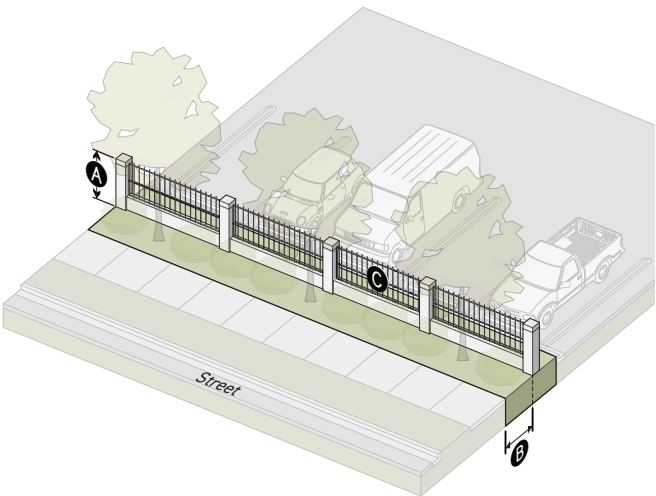
Intended for front and side street yards where the need for natural surveillance and visual interest is balanced with the need for separation between private ground floor uses and sidewalks.



DIMENSIONAL STANDARDS	
A Height (max)	3.5'
B Setback from lot line (min)	0'
Opacity (max)	50%

FENCE TYPE A4

Intended for front and side street yards where the need for natural surveillance and visual interest along the public realm is balanced with the need for security between private uses and the public realm.



DIMENSIONAL STANDARDS	
A Height (max)	8'
B Setback from lot line (min)	3'
C Opacity above 3' in height (max)	50%

#062

Posted by **Laurel David** on **02/03/2025** at **11:44am** [Comment ID: 1017] - [Link](#)

Question

Agree: 0, Disagree: 0

what is the difference between this and A2?

Reply by **SiteAdmin** on **02/06/2025** at **3:12pm** [Comment ID: 1038] - [Link](#)

Answer

Agree: 0, Disagree: 0

A2 is allowed in where fences and walls are prohibited, except when required for certain alcoholic uses. A3 allows them without this limitation.

#063

Posted by **Jim Winer** on **12/08/2024** at **3:24pm** [Comment ID: 660] - [Link](#)

Agree: 0, Disagree: 0

The illustration is of a fence with an opacity that is greater than 50%.

Reply by **SiteAdmin** on **12/09/2024** at **11:21am** [Comment ID: 701] - [Link](#)

Answer

Agree: 0, Disagree: 0

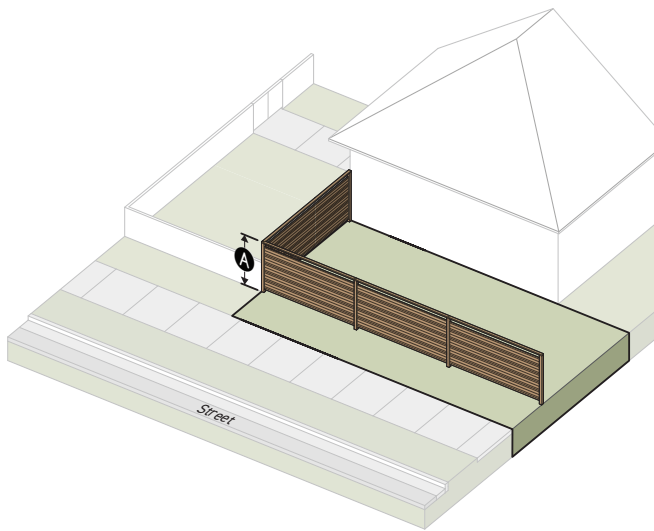
Thanks. We will fix this.

2. Side Street Yard Fence Types

The following fence types are intended for side street yards, along side street lot lines:

FENCE TYPE B1

Intended for side street street yards where natural surveillance and visual interest along the public realm is less critical than the need to mitigate impacts on private ground floor uses.



DIMENSIONAL STANDARDS

DIMENSIONAL STANDARDS	
A Height (max)	
	Within 3' of lot line
	More than 3' from lot line
	3.5'
	6'

064

#064

Posted by **Jim Winer** on **12/08/2024** at **3:28pm** [Comment ID: 661] - [Link](#)

Agree: 0, Disagree: 0

In fence types that are silent on opacity is one to assume that the fence / wall can be 100% opaque? Is there a distinction between a fence and a wall in this draft?

Reply by **SiteAdmin** on **12/09/2024** at **11:20am** [Comment ID: 700] - [Link](#)

Answer

Agree: 0, Disagree: 0

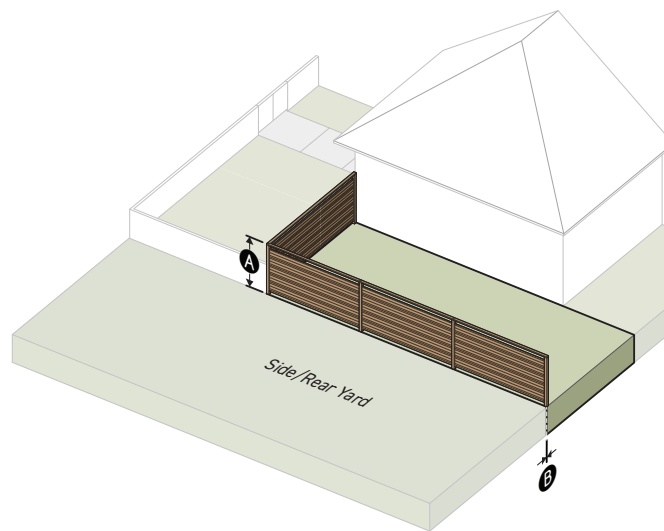
Correct. For the purpose of this code, a "Fence" and "wall" are the same.

3. Side or Rear Yard Fence Types

The following fence types are intended for side or rear yards, along common lot lines:

FENCE TYPE C1

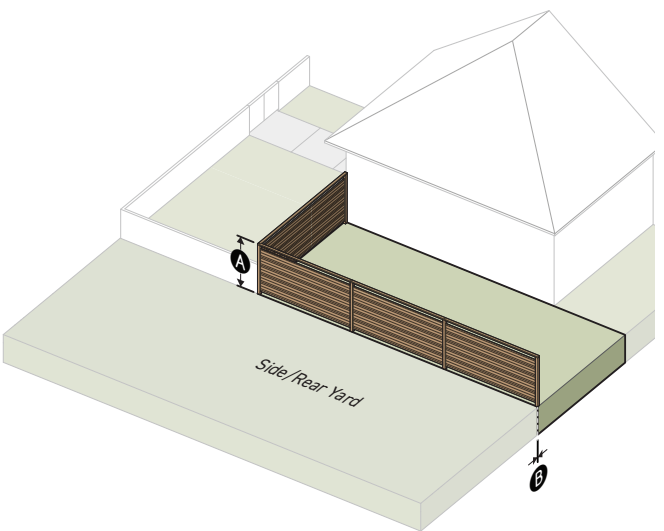
Intended for residential side and rear yards.



DIMENSIONAL STANDARDS	
A Height (max)	8'
B Setback from lot line (min)	0'

FENCE TYPE C2

Intended for heavy commercial and industrial side and rear yards.



DIMENSIONAL STANDARDS	
A Height (max)	8'
B Setback from lot line (min)	0'

4. Material

- a. Fences and walls must be constructed of durable, low maintenance material that has a long life expectancy.
- b. No fence or wall may be constructed of tires, junk, or other discarded materials.
- c. Fences and walls constructed of materials with a finished side must face the finished side toward the adjacent property.
- d. Chain-linked fences are not allowed in front or side street yards except in Workplace (W) and Park (PK) Form Districts. Where permitted, chain-linked fences must be vinyl-coated and are not allowed in a front yard or side street yard.
- e. Barbed wire or razer wire is not permitted.

5. Location

- a. Fences and walls must be set back from the lot line in accordance with **Sec. XX. Frontage Screens** and **Div. XX. Fences and Walls**.
- b. No fence or wall is allowed within any required drainage or utility easement.
- c. All fences and walls, including their sub-grade elements such as footings or foundation, must be located on-site.
- d. No fence or wall can obstruct the visibility of motorists, cyclists, and pedestrians at intersections or driveways.

6. Maintenance

All fences and walls must be maintained in good repair and must be kept vertical, structurally sound, and protected from deterioration.

D. Measurement

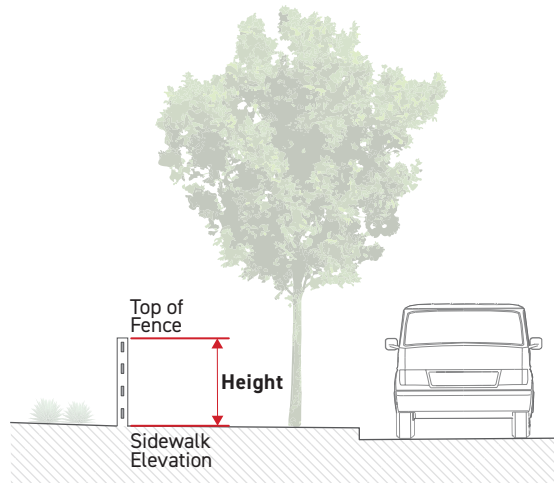
1. General

The allowable height of fences and walls is measured from existing grade.

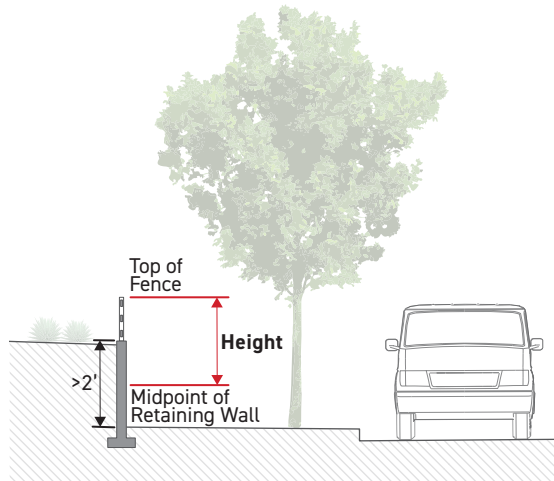
2. Front and Side Street Yards

For fences or walls in a front or side street yard, height is measured as follows:

- a. Fence or wall height is measured from the adjacent sidewalk to the topmost point of the fence or wall.



- b. Where no sidewalk exists within 20 feet of the fence or wall, height is measured from the base of the fence or wall to the topmost point of the fence or wall, on the exterior side of the fence or wall.
- c. Where a fence or wall is located within 3 feet of the exterior face of a retaining wall and the retaining wall is 2 feet in height or greater, the height is measured from the top of the fence or wall to the midpoint of the retaining wall. 065



3. Side and Rear Yards

For fences or walls in a side or rear yard, height is measured as follows:

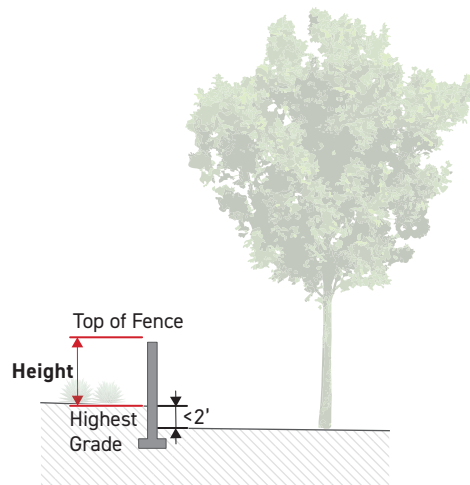
- a. Where the difference in grade on either side of a fence or wall is less than 2 feet, height is measured from the base of the wall on the side with the highest grade.

#065

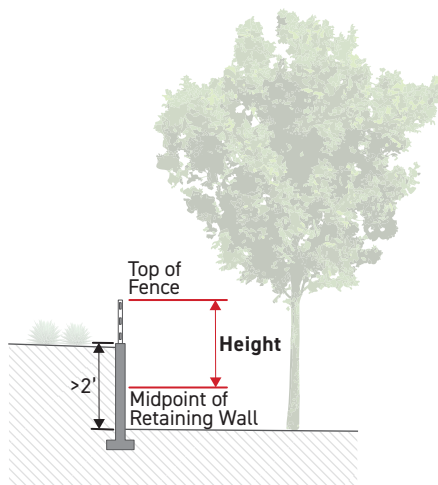
Posted by **Jim Winer** on **12/08/2024** at **3:22pm** [Comment ID: 659] - [Link](#)

Agree: 0, Disagree: 0

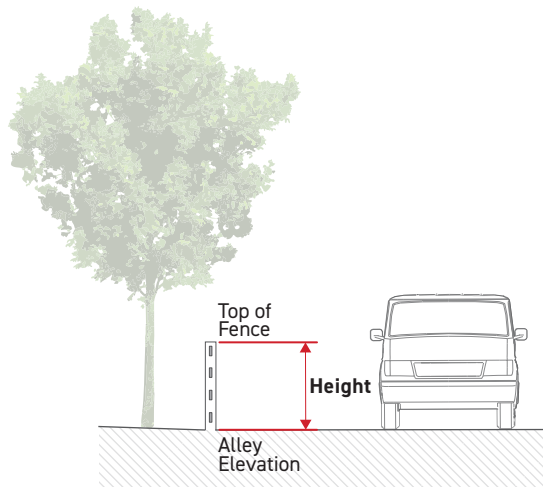
How does this work in cases where a wall that is 30" or taller requiring a 3'-0" or 3'-6" guard rail?



- b. Where a fence or wall is located within 3 feet of the exterior face of a retaining wall and the retaining wall is 2 feet in height or greater, the height is measured from the top of the fence or wall to the midpoint of the retaining wall.

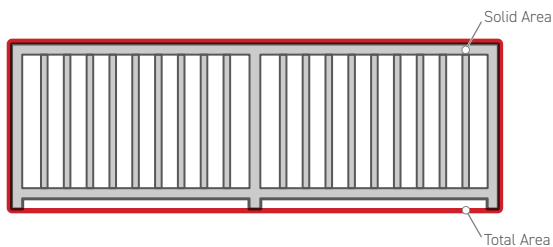


- c. Fences and walls abutting an alley in the rear or side yard are measured from existing grade at the adjacent alley and vertically to the topmost point of the fence or wall.



4. Opacity

- Opacity is measured as a percentage, calculated by dividing the solid portion of the fence or wall by the total area of the fence or wall.
- The total area of the fence or wall is measured as the smallest regular shape containing all elements of the fence or wall, excluding the top portions of finials or posts.



$$\frac{\text{Solid Area}}{\text{Total Area}} = \text{Opacity (\%)}$$

- If the opacity requirement applies to a portion of the fence or wall, the total area to be measured is limited to that specified portion.

E. Relief

- A change of up to 10% from fences and walls standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
- A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.

Sec. 8.4.6. Retaining Walls

A. Intent

To prevent retaining walls from looming over neighboring properties and public rights-of-way while improving the aesthetic quality of large retaining walls.

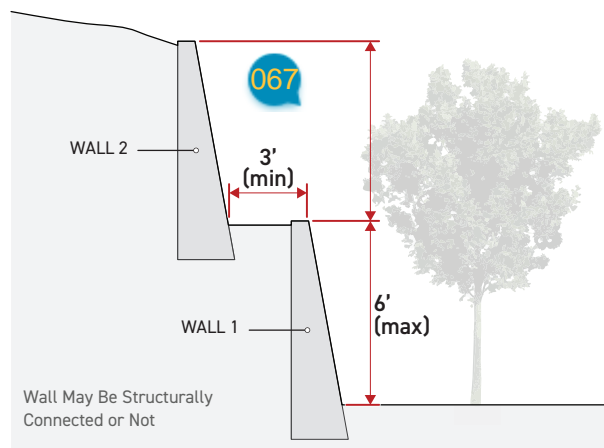
B. Applicability

Retaining wall standards apply to all lots.

C. Standards

1. General

- a. Retaining walls must not exceed a maximum height of 6 feet. However, two or more retaining walls can be built if they comply with the following standards:
 - i. The minimum horizontal distance between the walls is 3 feet; and
 - ii. None of the walls can exceed a height of 6 feet.



- b. Retaining walls located in a front or side street yard must not exceed a height of 2 feet.
- c. Retaining walls located in a rear or side yard must not exceed the height of a fence or wall type specified by the zoning district.
- d. No land-disturbing activity to construct a retaining wall may begin until a pre-construction meeting has taken place on-site between the City and the responsible party overseeing the installation and maintenance of erosion and sedimentation control measures.

2. Materials and Landscaping

- a. Retaining walls must be constructed from finished poured concrete faced with stone, brick, or smooth stucco.

- b. All retaining walls 5 feet or greater in height must be landscaped to be screened from view with screening plants, planted in accordance with Div. XX. Plants and Plant Material.

#066

Posted by **Louis Prevosti** on **01/22/2025** at **10:50am** [Comment ID: 928] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

Great requirement!

Reply by **SiteAdmin** on **01/23/2025** at **6:15pm** [Comment ID: 959] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thanks!

#067

Posted by **Jim Winer** on **12/08/2024** at **3:30pm** [Comment ID: 662] - [Link](#)

Agree: 0, Disagree: 0

What is the solution for retaining walls that require a guardrail? Which is any wall greater than 30". In the illustration, bith walls would likely require a guardrail.

Reply by **SiteAdmin** on **12/09/2024** at **11:16am** [Comment ID: 699] - [Link](#)

Answer

Agree: 0, Disagree: 0

Correct, depending on height and the width of the "step." Nothing in the zoning ordinance can supersede building code

#068

Posted by **Jennifer Friese** on **12/06/2024** at **6:50pm** [Comment ID: 655] - [Link](#)

Question

Agree: 0, Disagree: 0

Is this the existing standard?

Reply by **SiteAdmin** on **12/09/2024** at **11:27am** [Comment ID: 704] - [Link](#)

Answer

Agree: 0, Disagree: 0

Yes, but it's not in zoning - it's in Chapter 74 of the City Code. We are moving it to zoning.

Reply by **knorton123** on **01/13/2025** at **10:39am** [Comment ID: 881] - [Link](#)

Question

Agree: 0, Disagree: 0

Where in Chapter 74 is the max. wall height? I was not able to locate.

Reply by **SiteAdmin** on **01/13/2025** at **11:48am** [Comment ID: 884]

- [Link](#)

Answer

Agree: 0, Disagree: 0

See 74.32(11.1) which reads, The following provisions apply only to the R-1 through R-5 districts:(a)Imported material. Material that is deposited on-site from fill that is obtained from off-site is limited to 600 cubic yards.(b)Top of cut slope. The top of cut slope shall be set back at least four feet from any property line.(c)Toe of fill slope. The toe of fill slope shall be set back at least four feet from any property line.(d)Prohibition. Fill that is not restrained by a retaining wall shall not be undertaken on any part of a site where the natural slope is steeper than 2:1.(e)Retaining wall standards. Retaining walls in the buildable area of a site must:(i)Be set back from the property line at a distance that is at least equal to its height, where the height is measured from the ground level (existing),(ii)Not exceed a height of six feet, where the height is measured from the ground level (existing), and(iii)When tiered, the face of each upper retaining wall must be separated from the face of the nearest lower retaining wall by at least a minimum horizontal distance equal in length to the height of the upper retaining wall, where the height of the upper retaining wall is measured from the ground level after any grading has been carried out.

Note: The term "ground level (existing)" is defined in section 74-37 of the Code.

Note: The city Land Development Code contains controls on the construction and use of retaining walls in areas other than the buildable area.

Reply by **knorton123** on **01/13/2025** at **4:04pm** [Comment ID: 889]

- [Link](#)

Agree: 0, Disagree: 0

This excerpt from Section 74.32 only appears to apply to R-1 through R-5, and does not appear to cover all zoning classifications. The proposed 8.4.6.C. appears to affect all zoning classifications.

Reply by **SiteAdmin** on **01/16/2025** at **11:21am** [Comment ID: 898]

- [Link](#)

Answer

Agree: 0, Disagree: 0

Correct, the new standard technically DOES apply to all districts. However, there are standards in most Form Districts that specify how high buildings along the sidewalk may be raised. These are more restrictive than this provision and would supersede it.

#069

Posted by **Laurel David** on **02/03/2025** at **12:03pm** [Comment ID: 1019] - [Link](#)
Suggestion

Agree: 0, Disagree: 0

this appears to prohibit any retaining wall other than finished poured concrete. Commercial and industrial uses tend to use concrete block. Single family home owners would probably choose to construct from brick, stone or decorative concrete block

Reply by **SiteAdmin** on **02/06/2025** at **1:43pm** [Comment ID: 1033] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback.

#070

Posted by **Jennifer Frieese** on **12/06/2024** at **6:53pm** [Comment ID: 656] - [Link](#)

Suggestion

Agree: 1, Disagree: 0

This is a great requirement.

#071

Posted by **Louis Prevosti** on **01/22/2025** at **10:54am** [Comment ID: 929] - [Link](#)

Question

Agree: 0, Disagree: 0

It is my understanding that under the current zoning, a maximum of two 6 feet retaining wall can be built on a residential property. Is Zoning 2.0 now allowing more than this? How many? Unlimited?

Reply by **SiteAdmin** on **01/23/2025** at **6:10pm** [Comment ID: 958] - [Link](#)

Answer

Agree: 0, Disagree: 0

No change is proposed from existing standards found in Sec. 74-43(c)(11.1), which allows multiple walls but each cannot exceed 6 feet in height. It has been rewritten and graphics added.

D. Measurement

1. The allowable height of retaining walls is measured from existing grade.
2. The height of retaining walls is measured from the top of the wall to the lower side of the adjacent lowest existing grade on the outside of the wall.

E. Relief

1. A change of up to 10% from retaining wall standards may be granted in accordance with **Sec. XX. Administrative Adjustment.**
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance.**

Sec. 8.4.7. Plants and Plant Material

A. Intent

To support a healthy urban ecological system and provide the necessary supplemental information for all applicable planting and landscaping requirements in the Zoning Ordinance.

B. Applicability

The plants and plant material standards apply to any plant material used to meet a requirement of the Zoning Ordinance.

C. Standards

1. General

- a. All plants and plant material must meet the standards in **City of Atlanta Code, Chapter 158, Article II - Tree Protection**.
- b. Plant materials must be suited to the local environmental conditions of their specific planting location.
- c. Plant materials must be able to survive on natural rainfall once established with no loss of health, or an irrigation system must be provided.
- d. A natural-appearing dispersion and spacing of trees and shrubs throughout the site is encouraged.

2. Plant Materials Not Allowed

072

The Director may deem any plant material inappropriate because it is an invasive species or because the plant material is not well suited for the specific planting location. The planting of the following species are not allowed, except upon special review and approval by the Director:

- a. All plants identified as invasive or undesirable by the City Arborist in **City of Atlanta Code, Chapter 158, Article II - Tree Protection** are not permitted.
- 073 b. Columnar or fastigate species or cultivars cannot be used to meet landscape requirements unless the City Arborist determines that site conditions are unsuitable for a broad canopy tree.
- c. Larger shrub varieties can only be used to meet shrub landscape requirements and cannot be used to meet tree landscape requirements.
- d. No artificial trees or plants may be installed. Artificial turf is permitted.

#072

Posted by **Louis Prevosti** on **01/22/2025** at **10:58am** [Comment ID: 930] - [Link](#)

Question

Agree: 0, Disagree: 0

A COA arborist told me that Leyland Cypress is an invasive species, but many developers like to use it because it is an inexpensive way to create a green screen. Will Leyland Cypress be officially deemed invasive and not be allowed?

Reply by **SiteAdmin** on **01/23/2025** at **6:08pm** [Comment ID: 957] - [Link](#)

Answer

Agree: 0, Disagree: 0

Leland cypress will not be outlawed as part of the zoning code. This provision simply states that it may not be used to satisfy landscaping that is required by this code. Typically, required landscape includes street trees, buffer trees, etc. It does not include optional trees, such as on a private residential lot.

#073

Posted by **Louis Prevosti** on **01/22/2025** at **11:01am** [Comment ID: 931] - [Link](#)

Question

Agree: 0, Disagree: 0

Would Leyland Cypress be a columnar/fastigate species and thus not be allowed to be planted without the authorization of city arborists?

How will this be enforced?

Reply by **SiteAdmin** on **01/23/2025** at **6:06pm** [Comment ID: 956] - [Link](#)

Answer

Agree: 0, Disagree: 0

Please note, this only requires to landscaping that is REQUIRED by the zoning ordinance, such as street tree plantings and required transitions (buffered); it would not apply to additional optional plantings. All of these types of activities already do, and will continue to, require landscape plans as part of permitting.

3. Planting Requirements

a. Planting Size

All landscape planting materials must conform to the minimum size or height standards in the following table, and meet the standards of the *American Standard for Nursery Stock (ANSI Z-60.1)* at the time of planting.

Planting Material	Min. Size and Height	
	Planting	Maturity
Trees		
Canopy tree	3" caliper / 12' height	30' canopy spread
Understory trees	2" caliper / 10' height	15' to 30' canopy spread
Multi-stemmed trees	5 canes max / 10' height	15' to 30' canopy spread
Screening shrubs		
Evergreen shrubs	18" height	3' height
Deciduous shrubs	2' height	3' height
Other screening plants (evergreen)	4' height	10' max width / 8' height

b. Trees

- i. All planting areas must meet the minimum soil volume requirements for tree size as specified in the *Chapter 158, Article II – Tree Protection*. Only trees with adequate soil volumes will be counted toward the minimum tree canopy cover requirement.
- ii. Trees in planting areas less than 135 square feet must be of a species known to thrive in low soil volume areas.
- iii. Unless approved by the Director, only trees with a mature height of less than 20 feet may be installed under overhead utility lines.

4. Installation

- a. Plant material must be installed according to *American National Standards Institute ANSI A300 tree care standards*. This includes the removal of straps, burlap wraps, cutting of wire baskets, and proper mulch techniques.
- b. All planting areas must be protected from vehicle damage by the installation of curbing or other methods approved by the Director. Alternative barrier designs which provide improved infiltration or storage of stormwater are strongly encouraged.
- c. The plant materials used in and around parking lots and adjacent to public rights-of-way and pedestrian ways must have a height at maturity designed to maintain visibility at intersections to ensure the safety of pedestrians.
- d. All planting areas must be stabilized with ground covers, mulches, or other materials approved by the Director to prevent soil erosion and to allow rainwater infiltration. Rubber mulch is not acceptable.

#074

Posted by **Laurel David** on **01/24/2025** at **3:34pm** [Comment ID: 974] - [Link](#)

Question

Agree: 0, Disagree: 0

Has this section been synchronized with the new tree preservation ordinance?

Reply by **SiteAdmin** on **01/28/2025** at **5:09pm** [Comment ID: 993] - [Link](#)

Answer

Agree: 0, Disagree: 0

Yes. We have coordinated with the Arborist Division. If the TPO is adopted with a different standard, we will modify these to match it.

#075

Posted by **Laurel David** on **01/24/2025** at **3:37pm** [Comment ID: 975] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

I doubt GA Power will allow the tree height to be increased, even if the Director will. Ga Power should be consulted

Reply by **SiteAdmin** on **01/28/2025** at **5:12pm** [Comment ID: 994] - [Link](#)

Answer

Agree: 0, Disagree: 0

Agreed. They will not typically, but where there are extremely high transmission lines it is sometimes an option.

5. Maintenance

- a. Plant materials must be maintained in good and healthy condition.
- b. Planting areas must be kept free of weeds and trash.
- c. All plants and materials used to comply with this section must be arranged to ensure easy access for maintenance, maintain clear sight lines, avoid encroaching on neighboring properties, and consist of species appropriate for the proposed location, taking into account any conflicts with utility easements and rights-of-way.

D. Measurement

For the measurement of caliper, height, and canopy spread, see the "Methods of Measurement" section of the latest version of the *American Standard for Nursery Stock (ANSI Z-60.1)*.

E. Relief

1. A change of up to 10% from plants and plant material standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.
3. An alternative landscape plan designed by a licensed Landscape Architect that meets the intent of the landscape standards to an extent equal to or better than the specified design standards may be considered by the Director.

#076

Posted by **Laurel David** on **02/03/2025** at **12:08pm** [Comment ID: 1020] - [Link](#)

Suggestion

Agree: 0, Disagree: 0

The City Arborist has final say on changing anything covered by the tree protection ordinance

Reply by **SiteAdmin** on **02/06/2025** at **1:19pm** [Comment ID: 1032] - [Link](#)

Answer

Agree: 0, Disagree: 0

Thank you for your feedback. We will be updating various provision regarding coordination with other City departments.

SIGNS

DIVISION 8.5. **SIGNS**

[Note: Section forthcoming in Module III]

DIVISION 8.6. **LIGHTING**

Sec. 8.6.1. **Outdoor Lighting**

A. Intent

To provide exterior lighting standards that support a variety of environments and minimize the negative impacts of lighting on adjacent uses and users of the public realm.

B. Applicability

1. New Fixtures

Outdoor lighting standards apply to all exterior lighting fixtures on any lot installed after the effective date of the Zoning Ordinance.

2. Existing Fixtures

- a. Routine maintenance, including changing the lamp, ballast, starter, photo control, fixture housing, lens and other required components, is allowed for all existing fixtures.
- b. The installation of new site lighting, replacement of existing lighting, and any modifications to light fixture wattage, fixture type, mounting, or fixture location must comply with the outdoor lighting standards.

3. Exemptions

Lighting required by the Federal Aviation Administration does not have to comply with the outdoor lighting standards.

C. Standards

1. Prohibited Lighting Sources

The following light fixtures and sources cannot be used:

- a. Cobra-head-type fixtures with dished or drop lenses or refractors, which contain sources that are not LED. 077
- b. Temporary searchlights and other high-intensity narrow-beam moving fixtures that shine light directly up to the sky.
- c. Blinking, flashing, or fluttering lights or other illuminated device that has a changing light intensity, brightness, or color, except for temporary holiday displays or as allowed under **Sec. XX. Signs**.
- d. Any lights directed, reflected, or with colors that may create a hazard to operators of motor vehicles or to operators of aircrafts.

2. Design and Installation 078

- a. Light sources must be concealed or shielded to minimize the potential for direct glare and lighting trespassing onto on adjacent properties and rights-of-way.

#077

Posted by **Jim Winer** on **12/08/2024** at **4:16pm** [Comment ID: 667] - [Link](#)

Agree: 0, Disagree: 0

I don't understand what the intent is here. I could understand a specific light cut off / light bleed / FC limitation (ie City of Alpharetta's) and maybe an energy efficiency requirement. Does this mean a Cobra-head is fine as long as it is lamped with an LED? What happens when a more efficient lighting technology is developed that supplants LEDs?

Reply by **SiteAdmin** on **12/09/2024** at **11:07am** [Comment ID: 694] - [Link](#)

Answer

Agree: 0, Disagree: 0

We will clarify this. Thanks!

#078

Posted by **Jim Winer** on **12/08/2024** at **4:17pm** [Comment ID: 668] - [Link](#)

Agree: 0, Disagree: 0

Recommend that you look at the City of Alpharetta lighting requiriements.

Reply by **SiteAdmin** on **12/09/2024** at **11:06am** [Comment ID: 693] - [Link](#)

Answer

Agree: 0, Disagree: 0

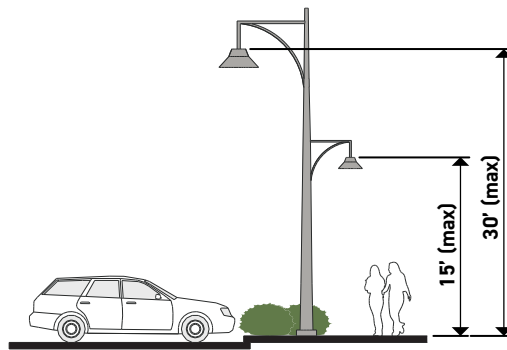
Thanks. Will do!

LIGHTING

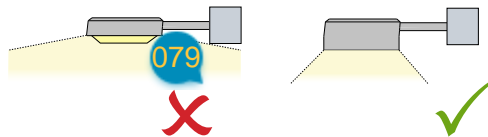
- b. The maximum light level of any light fixture measured at a lot line cannot exceed an average of 2.0 footcandles. The uniformity ratio should be no more than 4:1.
- c. Light sources must use a color temperature of no more than 4,500 Kelvin, with a Color Rendering Index (CRI) value of 80 or higher.
- d. Lighting must not be oriented onto adjacent properties, sidewalks, or streets.
- e. Service connections for all freestanding lighting fixtures must be installed underground.

3. Parking and Pedestrian Area Lighting

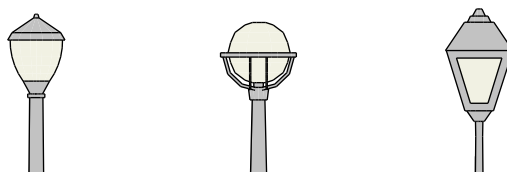
- a. All light fixtures must be full cutoff or shielded, except as listed below.
- b. Light fixtures within parking lots and vehicle use areas can be no higher than 30 feet.
- c. Light fixtures within pedestrian areas can be no higher than 15 feet.



- d. All parking lots and structures must provide a minimum illumination of 2.0 footcandles.



- e. Non-cutoff or un-shielded fixtures can be used if the maximum initial lumens generated by each fixture is less than 9,500. These fixtures generally feature globes or vertical glass planes and must be coated with an internal white frosting to diffuse light.



#079

Posted by **Jim Winer** on **12/08/2024** at **4:19pm** [Comment ID: 669] - [Link](#)

Agree: 0, Disagree: 0

Diagram lacks specificity and has little or no meaning. Would a light angle between the 2 be OK? I suggest a specific requirement like light bleed from a property line can not exceed 1FC and must be zero within X feet.

Reply by **SiteAdmin** on **12/09/2024** at **11:05am** [Comment ID: 692] - [Link](#)

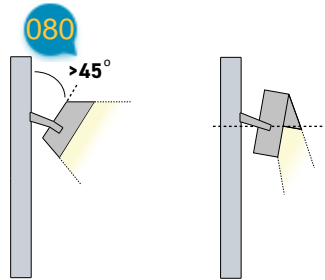
Answer

Agree: 0, Disagree: 0

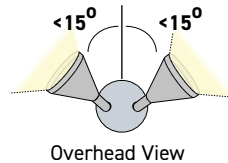
Thank you. We will clarify this in the next version.

4. Flood Lights and Flood Lamps

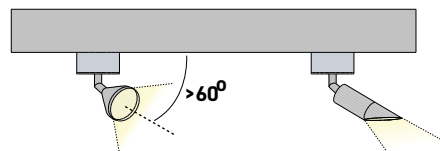
- a. Flood light fixtures must be aimed down at least 45 degrees from a vertical plane or the front of the fixture must be shielded so that no portion of the light bulb extends below the bottom edge of the shield.



- b. Flood light fixtures located within 50 feet of a public right-of-way must be mounted and aimed perpendicular to the right-of-way, with a side-to-side horizontal aiming tolerance of no more than 15 degrees.



- c. All flood lamps emitting 1,000 or more lumens must be shielded or aimed at least 60 degrees down from the horizontal plane so that the main beam is not visible from adjacent properties or the public right-of-way.

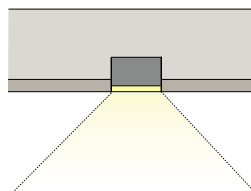


5. Awnings and Canopies

Lighting under awnings or canopies must be less than 24 maintained footcandles and be designed to prevent glare off-site. Acceptable lighting designs include the following:

a. Recessed

Recessed fixture incorporating a lens cover that is either recessed or flush with the bottom surface of the awning or canopy.



#080

Posted by **haney8604** on **12/17/2024** at **4:55pm** [Comment ID: 769] - [Link](#)

Agree: 0, Disagree: 0

This diagram may be misinterpreted based on a specific light fixture. Suggest providing angle from the center of the beam to the horizontal plane instead - or to the edge of the beam as is done in some other diagrams here.

Reply by **SiteAdmin** on **12/18/2024** at **1:43pm** [Comment ID: 787] - [Link](#)

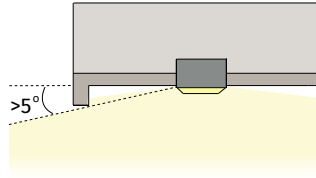
Answer

Agree: 0, Disagree: 0

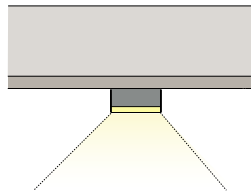
Thanks for the suggestion.

LIGHTING**b. Shielded**

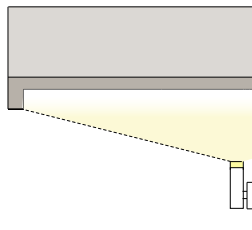
Light fixture incorporating shields or is shielded by the edge of the awning or canopy itself, so that light is restrained to 5 degrees or more below the horizontal plane.

**c. Surface Mounted**

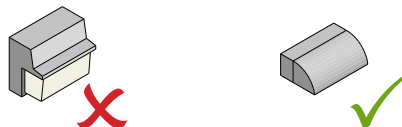
Surface mounted fixture incorporating a flat glass that provides a cutoff design or shielded light distribution.

**d. Indirect**

Indirect lighting where light is beamed upward and then reflected down from the underside of the awning or canopy, provided the fixture is shielded so that direct illumination is focused exclusively on the underside of the awning or canopy.

**6. Building Lighting**

- a. Lighting fixtures must be selected, located, aimed and shielded so that direct illumination is focused exclusively on the building facade, plantings and other intended site features and away from adjoining properties and the public right-of-way.
- b. All wall pack fixtures must be full cutoff fixtures.



- c. Only lighting used to accent architectural features, landscape, or art may be directed upward, provided that the fixture is located, aimed or shielded to minimize light spill into the night sky.

D. Measurement

1. Light levels are specified, calculated and measured in footcandles. All footcandles values are maintained footcandles.
2. Measurements are to be made at ground level, with the light-registering portion of the meter held parallel to the ground pointing up.

E. Relief

1. A change of up to 10% from outdoor lighting standards may be granted in accordance with **Sec. XX. Administrative Adjustment**.
2. A change beyond 10% may be granted in accordance with **Sec. XX. Variance**.